

PROJECT CONSTRUCTION

Project construction would take place in three stages. The exact locations and timing of activities would not be determined until closer to construction start to allow for a flexible and efficient schedule. Additional information will be provided at a preconstruction public meeting anticipated for 2023.

STAGE 1 would consist of the demolition of the unused third bridge bay on the west and construction of retaining walls on the west side of railroad property.

STAGE 2 would consist of the construction of the new northbound bridges and tracks west of the current tracks and demolition of the existing northbound bridge bay.

STAGE 3 would consist of the construction of new southbound bridges and tracks, demolition of the old southbound bridge bay, and construction of retaining walls on the east side of railroad property (where required).

CONCEPTUAL RENDERINGS OF ROSCOE ST.

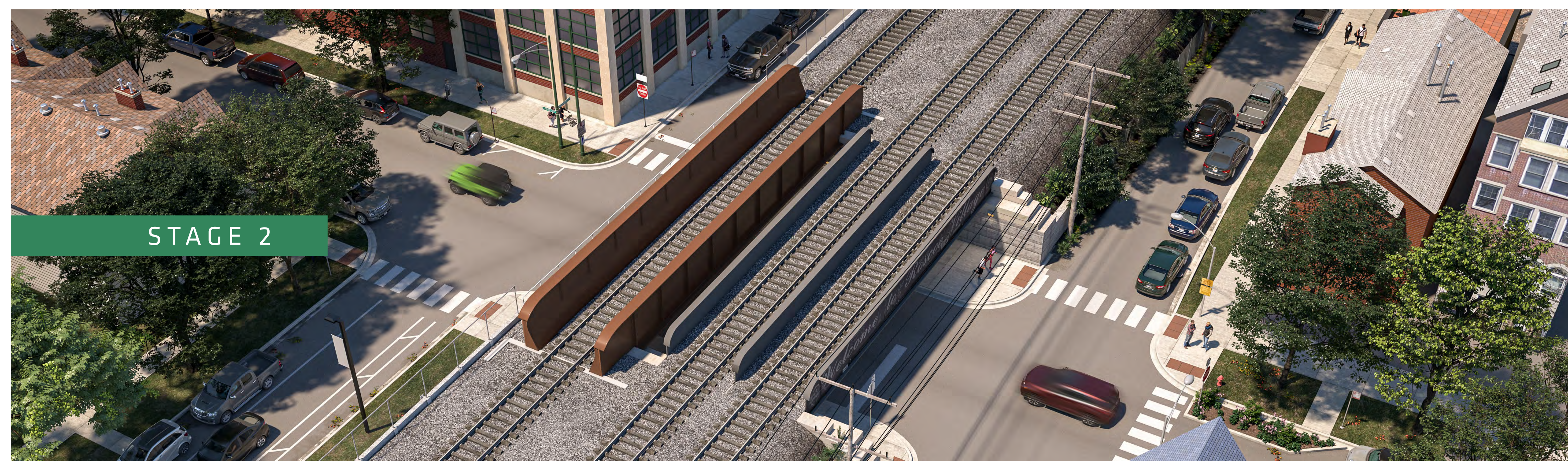
EXISTING CONDITIONS

STAGE 1

STAGE 2

STAGE 3

FINAL CONDITIONS



ANTICIPATED COMMUNITY IMPACTS & CHANGES

- Temporary street, sidewalk, bike lane, and public parking spot closures
- Temporary bus reroutes and bus stop closures or relocations
- Staged construction equipment
- Temporary increased levels of noise, vibration, dust, and other impacts near construction
- Removal of some community features including signage, art installations, and landscaped areas

METRA'S COMMITMENT

To minimize community impacts, Metra plans to

- Notify the public of upcoming work and closures, including a preconstruction meeting prior to the start of work
- Maintain traffic circulation with clearly marked temporary detours and roadway modifications
- Maintain sidewalk access on at least one side of the roadway, where practicable
- Restore roadways and bike lanes per CDOT requirements following construction
- Coordinate with CTA to determine bus reroutes and stop modifications
- Coordinate with aldermanic offices to minimize temporary public parking impacts
- Coordinate with stakeholders to consider options to retain or restore community features, signage, gardens, and vegetation within the public way
- Use best practices to minimize noise, vibration, dust, and other temporary impacts, where feasible

ANTICIPATED ADJACENT PROPERTY IMPACTS & CHANGES

- Required construction work near the railroad property line to accommodate the track shift and new retaining walls, particularly west of the existing tracks
- Temporary construction easements may be required to allow construction workers access to complete excavation, retaining wall installation, fencing, and collection of construction debris accumulated during activities
- Items on railroad property will need to be removed before construction

METRA'S COMMITMENT

To minimize community impacts, Metra plans to

- Determine the locations of temporary easements as part of the design process and communicate with property owners prior to construction
- Minimize the need for temporary easements on private property and restore areas following construction; any temporary easement needs would require a legal agreement with private property owners
- Complete construction and stage construction equipment predominantly on railroad property
- Assign an Adjacent Property Liaison to meet with residents, provide information on proposed changes, and address questions and comments about the Project

ANTICIPATED VEGETATION IMPACTS & CHANGES

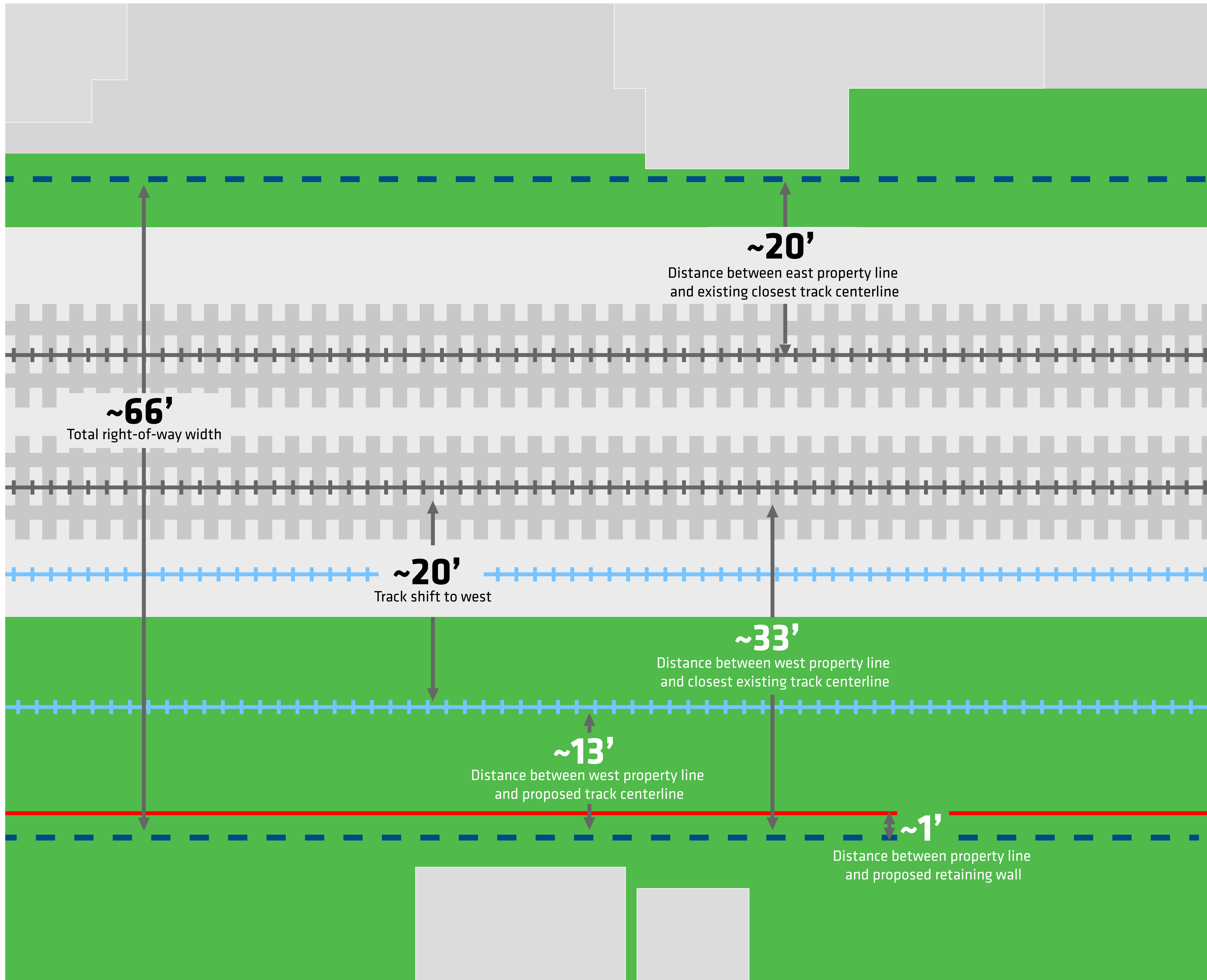
- Removal of trees and vegetation on railroad property
- Limited removal on adjacent public property, primarily near the railroad property line
- Limited removal on adjacent private property, exclusively where temporary easement locations are needed

METRA'S COMMITMENT

To minimize vegetation impacts, Metra plans to

- Determine locations of tree and vegetation removal as part of the design process and communicate locations to the community prior to construction
- Minimize impacts to vegetation where feasible and implement best practices to avoid incidental impacts
- Develop a landscape plan for adjacent public way; opportunities will be based on available space, obstructions, agency coordination, and community input

ADJACENT PROPERTIES TO THE EAST



ADJACENT PROPERTIES TO THE WEST



UP North Rebuild:

Fullerton to Addison Project

TRACK ADJUSTMENT MAP

Legend

- Dimensions (feet)
- +++ Existing Track Centerlines (to be removed)
- Proposed Retaining Wall (preliminary location)
- +++ Proposed Track Centerlines (preliminary location)
- - Union Pacific Right-Of-Way

Note: This map depicts a typical location. Dimensions vary throughout the corridor.



CONSTRUCTION OPTIONS CONSIDERED

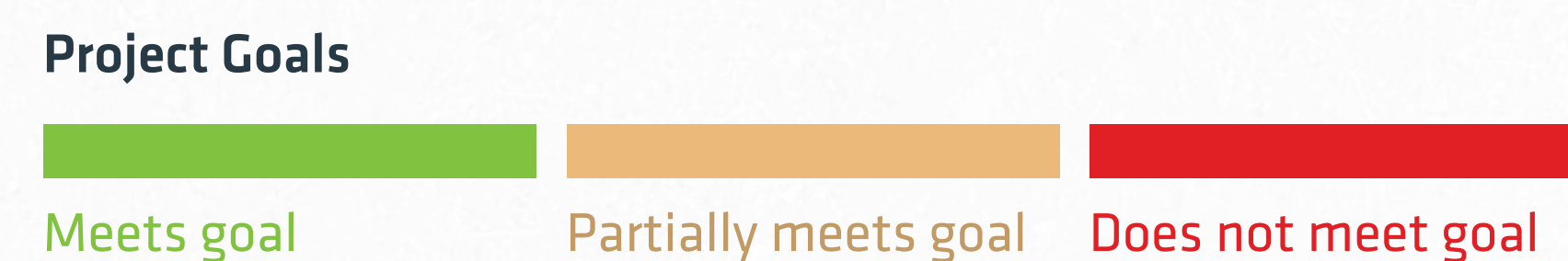
Needs of the Project:

- 1) Modernize infrastructure,
- 2) Create efficiencies and reduce operating costs, and
- 3) Improve the customer experience along the UP North Line

The **Purpose of the Project** is to replace 11 bridges and retaining walls, refurbish one (1) bridge, and complete other track improvements between Fullerton Avenue and Addison Street.

Metra considered a No Build option and five (5) other options to reconstruct the existing bridges. The East Track Shift option has been added for Metra's consideration after community feedback. These options were evaluated based on how they meet the goals of the Project and help minimize potential impacts. All options include the replacement of the 11 bridges and retaining walls as they have exceeded their design life. Note: The height of retaining walls needed may vary based on the construction option.

Construction Option	PROJECT GOALS					ANTICIPATED IMPACTS					
	Need and Purpose	Maintain Train Operations	Minimize Costs	Engineering and Construction Feasibility	Construction Duration	Views from Adjacent Residences	Property Acquisition	Traffic	Vegetation Removal	Noise and Vibration	Parks and Public Lands
No Build			N/A		N/A	N/A	N/A	N/A	N/A	N/A	N/A
Track Shift West - "Preferred"											
Track Shift East											
Single-Track Operations											
Temporary "Shoofly" Track											
Bridge Roll-In											



UP NORTH REBUILD PROJECT CORRIDOR

WAVELAND AVE

N RAVENSWOOD AVE

ADDISON ST

LINCOLN AVE

N RAVENSWOOD AVE

W PATTERSON AV

- Existing Tracks
- Proposed Retaining Walls (Preliminary Location)
- Proposed Tracks (Preliminary Location)
- Union Pacific ROW

0 100 150 200 300 Feet



GRACE ST.
TO ADDISON ST.



CORNELIA AVE.
 TO SCHOOL ST.



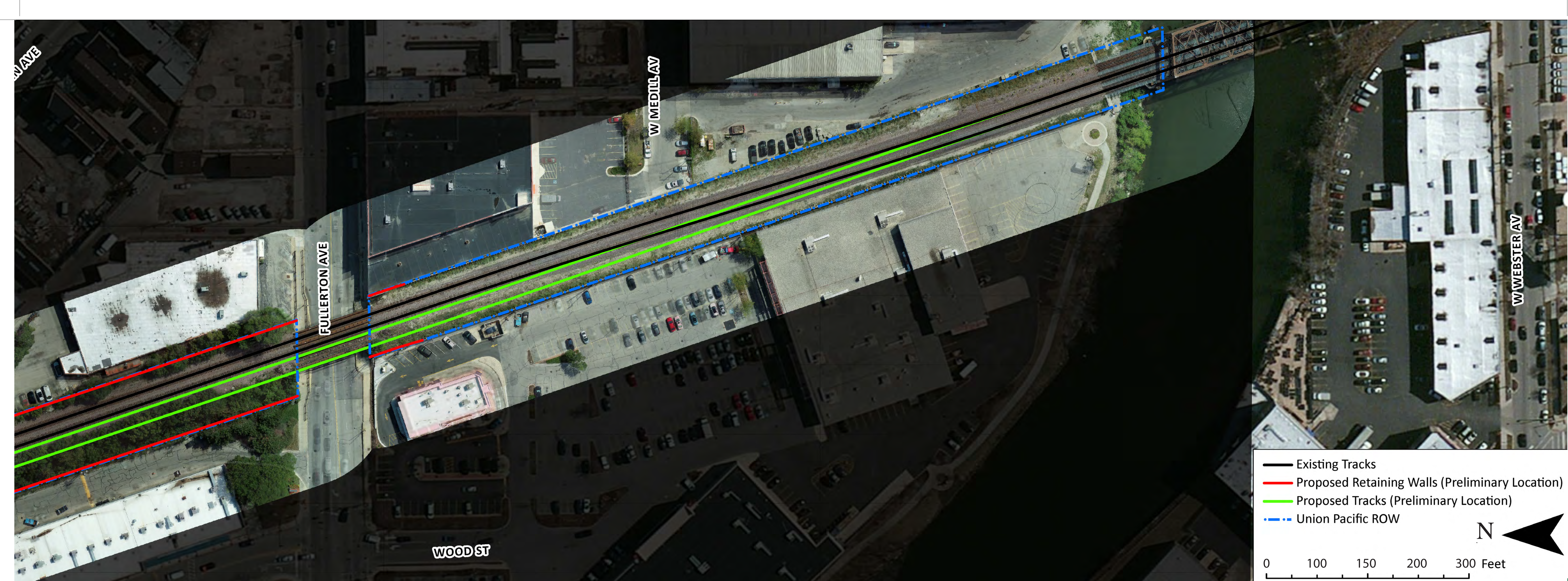
MELROSE ST. TO WELLINGTON AVE.



WELLINGTON AVE. TO DIVERSEY AVE.



NORTH OF WRIGHTWOOD AVE.
TO SOUTH OF CLYBOURN AVE.



FULLERTON AVE. TO NORTH BRANCH CHICAGO RIVER