

Appendix D

Hazardous Materials Supporting Documentation

Appendix D-2: Historic Aerials



NEPA-BRIDGES

NEPA-BRIDGES

Chicago, IL 60657

Inquiry Number: 6537815.8

June 17, 2021

The EDR Aerial Photo Decade Package



6 Armstrong Road, 4th floor
Shelton, CT 06484
Toll Free: 800.352.0050
www.edrnet.com

Date EDR Searched Historical Sources:

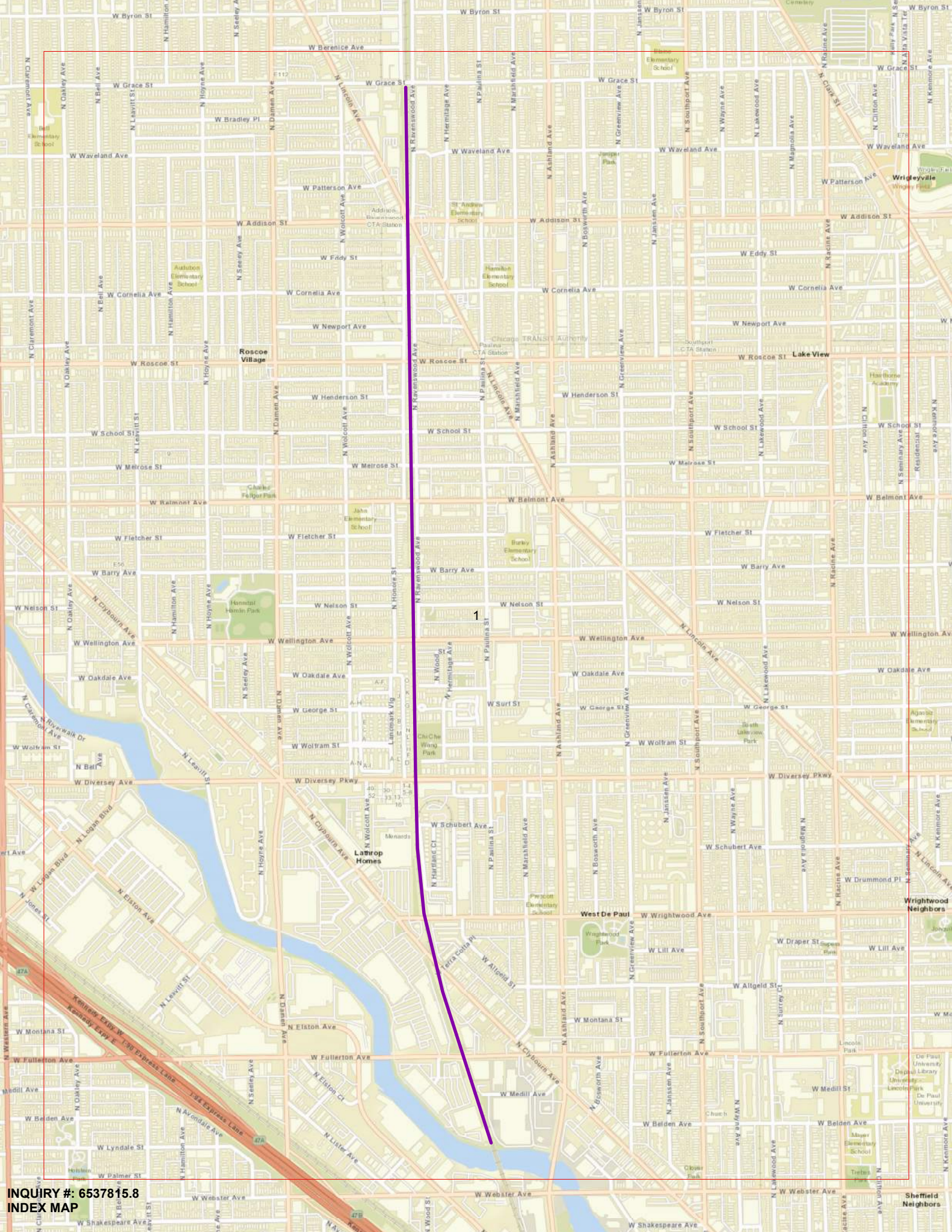
Aerial Photography June 15, 2021

Target Property:

NEPA-BRIDGES

Chicago, IL 60657

<u><i>Year</i></u>	<u><i>Scale</i></u>	<u><i>Details</i></u>	<u><i>Source</i></u>
1938	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1938	ILGS
1952	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1952	USGS
1962	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1962	USGS
1972	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1972	USGS
1984	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1984	USDA
1999	Aerial Photograph. Scale: 1"=1000'	Flight Year: 1999	USGS/DOQQ
2007	Aerial Photograph. Scale: 1"=1000'	Flight Year: 2007	USDA/NAIP
2009	Aerial Photograph. Scale: 1"=1000'	Flight Year: 2009	USDA/NAIP
2012	Aerial Photograph. Scale: 1"=1000'	Flight Year: 2012	USDA/NAIP
2017	Aerial Photograph. Scale: 1"=1000'	Flight Year: 2017	USDA/NAIP



1

INQUIRY #: 6537815.8
INDEX MAP

Sheffield Neighbors



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SCALE: 1"=1000'



INQUIRY #: 6537815.8
YEAR: 1952



INQUIRY #: 6537815.8
YEAR: 1962
SCALE: 1"=1000'



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SCALE: 1"=1000'



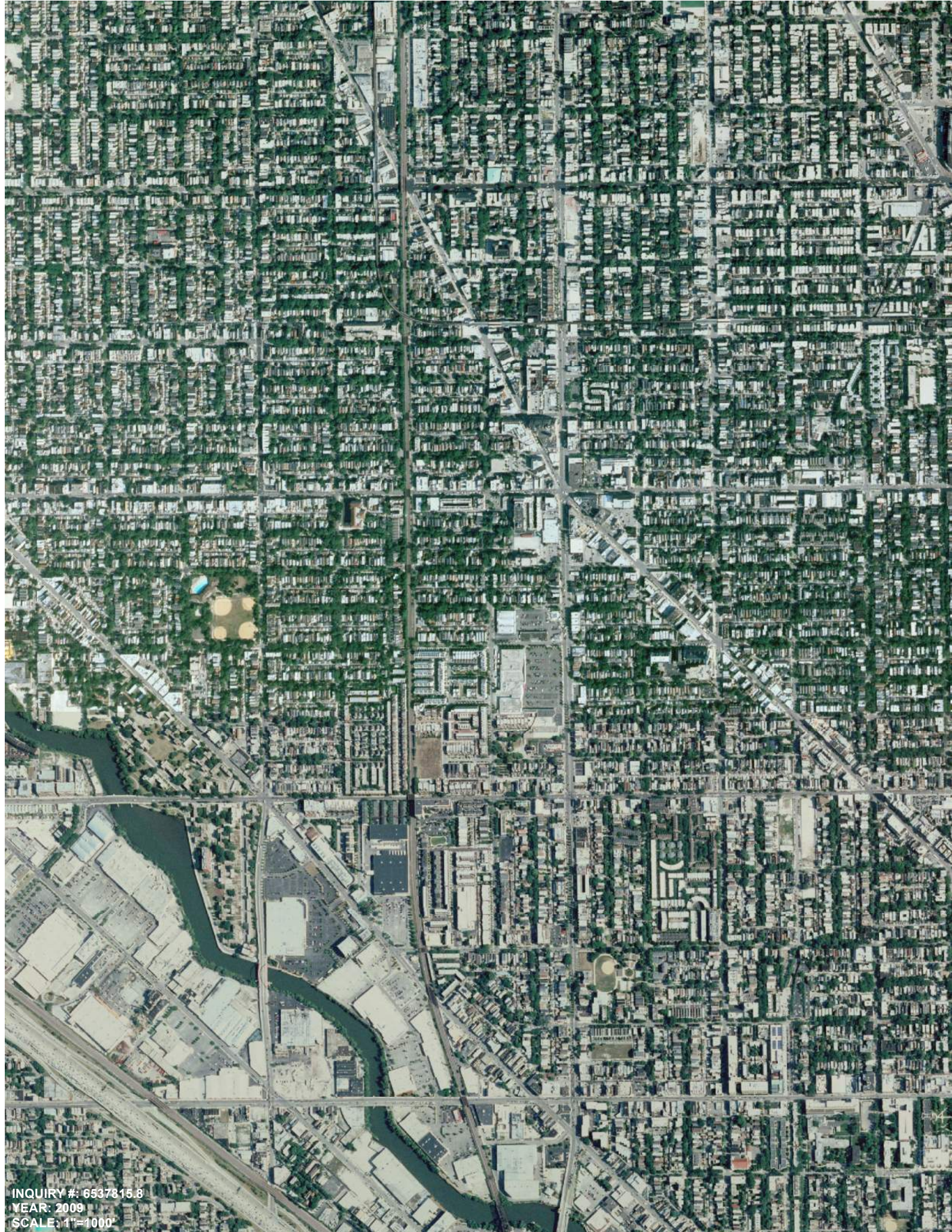
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INQUIRY #: 6537815.8
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INQUIRY# 6587815.8
YEAR: 2012
SCALE: 1"=1000'



INQUIRY #: 6537815.8
YEAR: 2017
SCALE: 1"=1000'

Appendix D

Hazardous Materials Supporting Documentation

Appendix D-3: Historic Topographic Maps

NEPA-BRIDGES

NEPA-BRIDGES

Chicago, IL 60657

Inquiry Number: 6537815.5

June 16, 2021

EDR Historical Topo Map Report

with QuadMatch™



6 Armstrong Road, 4th floor
Shelton, CT 06484
Toll Free: 800.352.0050
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EDR Historical Topo Map Report

06/16/21

Site Name:

NEPA-BRIDGES
NEPA-BRIDGES
Chicago, IL 60657
EDR Inquiry # 6537815.5

Client Name:

CDM Smith Inc.
125 S. Wacker Drive
Chicago, IL 60606
Contact: Handssl@cdmsmith.com



EDR Topographic Map Library has been searched by EDR and maps covering the target property location as provided by CDM Smith Inc. were identified for the years listed below. EDR's Historical Topo Map Report is designed to assist professionals in evaluating potential liability on a target property resulting from past activities. EDR's Historical Topo Map Report includes a search of a collection of public and private color historical topographic maps, dating back to the late 1800s.

Search Results:

Coordinates:

P.O.# NA
Project: NA

Latitude: 41.934778 41° 56' 5" North
Longitude: -87.673417 -87° 40' 24" West
UTM Zone: Zone 16 North
UTM X Meters: 444172.15
UTM Y Meters: 4642754.00
Elevation: 600.00' above sea level

Maps Provided:

2012	1901
1997	1900
1993	1891
1978	1889
1972	
1963	
1953	
1929	

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Topo Sheet Key

This EDR Topo Map Report is based upon the following USGS topographic map sheets.

2012 Source Sheets



Chicago Loop
2012
7.5-minute, 24000

1997 Source Sheets



Chicago Loop
1997
7.5-minute, 24000
Aerial Photo Revised 1988

1993 Source Sheets



Chicago Loop
1993
7.5-minute, 24000
Aerial Photo Revised 1988

1978 Source Sheets



Chicago Loop
1978
7.5-minute, 24000
Aerial Photo Revised 1972

Topo Sheet Key

This EDR Topo Map Report is based upon the following USGS topographic map sheets.

1972 Source Sheets



Chicago Loop
1972
7.5-minute, 24000
Aerial Photo Revised 1972

1963 Source Sheets



Chicago Loop
1963
7.5-minute, 24000
Aerial Photo Revised 1962

1953 Source Sheets



Chicago Loop
1953
7.5-minute, 24000

1929 Source Sheets



Chicago Loop
1929
7.5-minute, 24000

Topo Sheet Key

This EDR Topo Map Report is based upon the following USGS topographic map sheets.

1901 Source Sheets



Chicago
1901
15-minute, 62500

1900 Source Sheets



Chicago
1900
15-minute, 62500

1891 Source Sheets

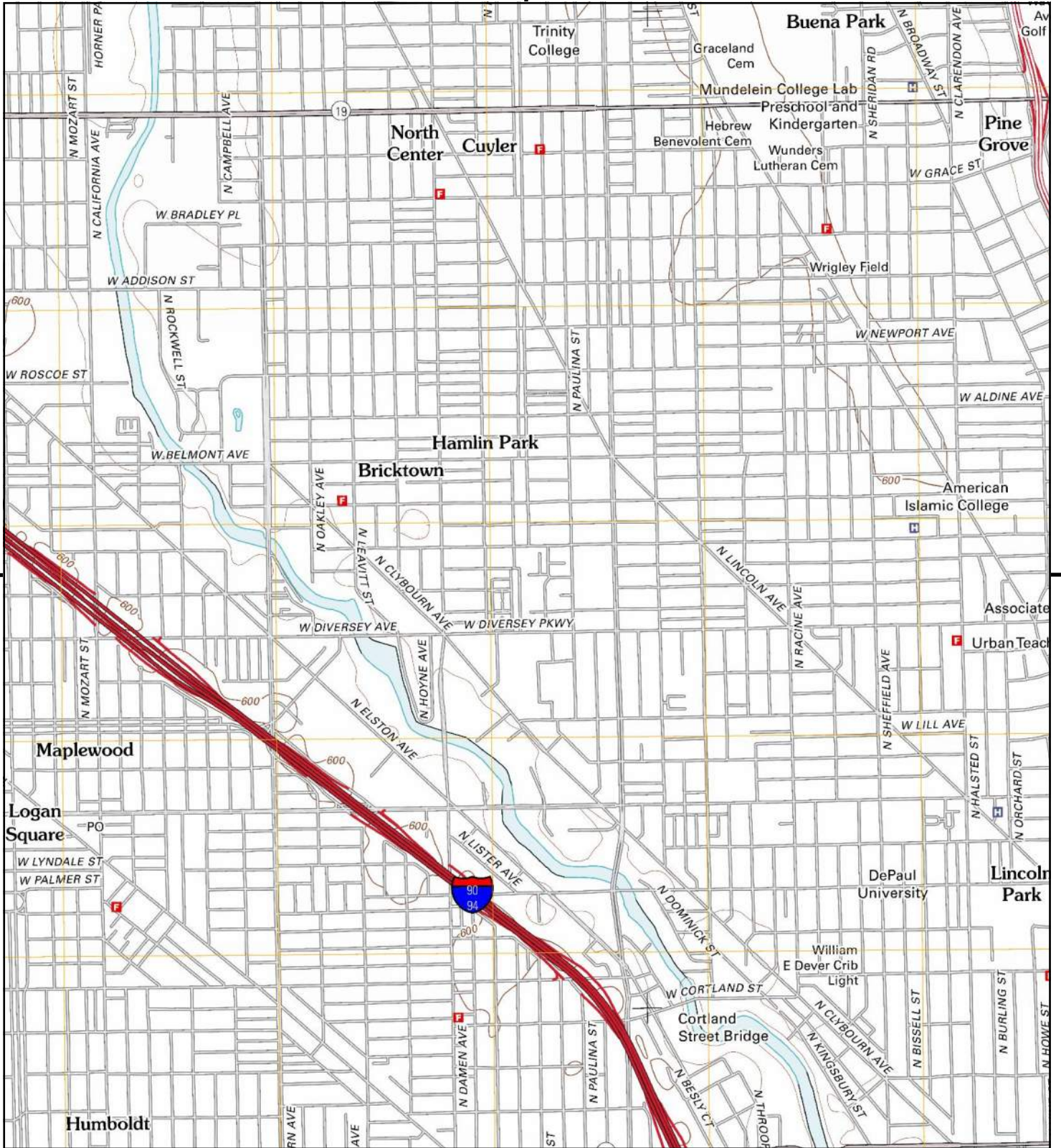


Chicago
1891
15-minute, 62500

1889 Source Sheets



Chicago
1889
15-minute, 62500



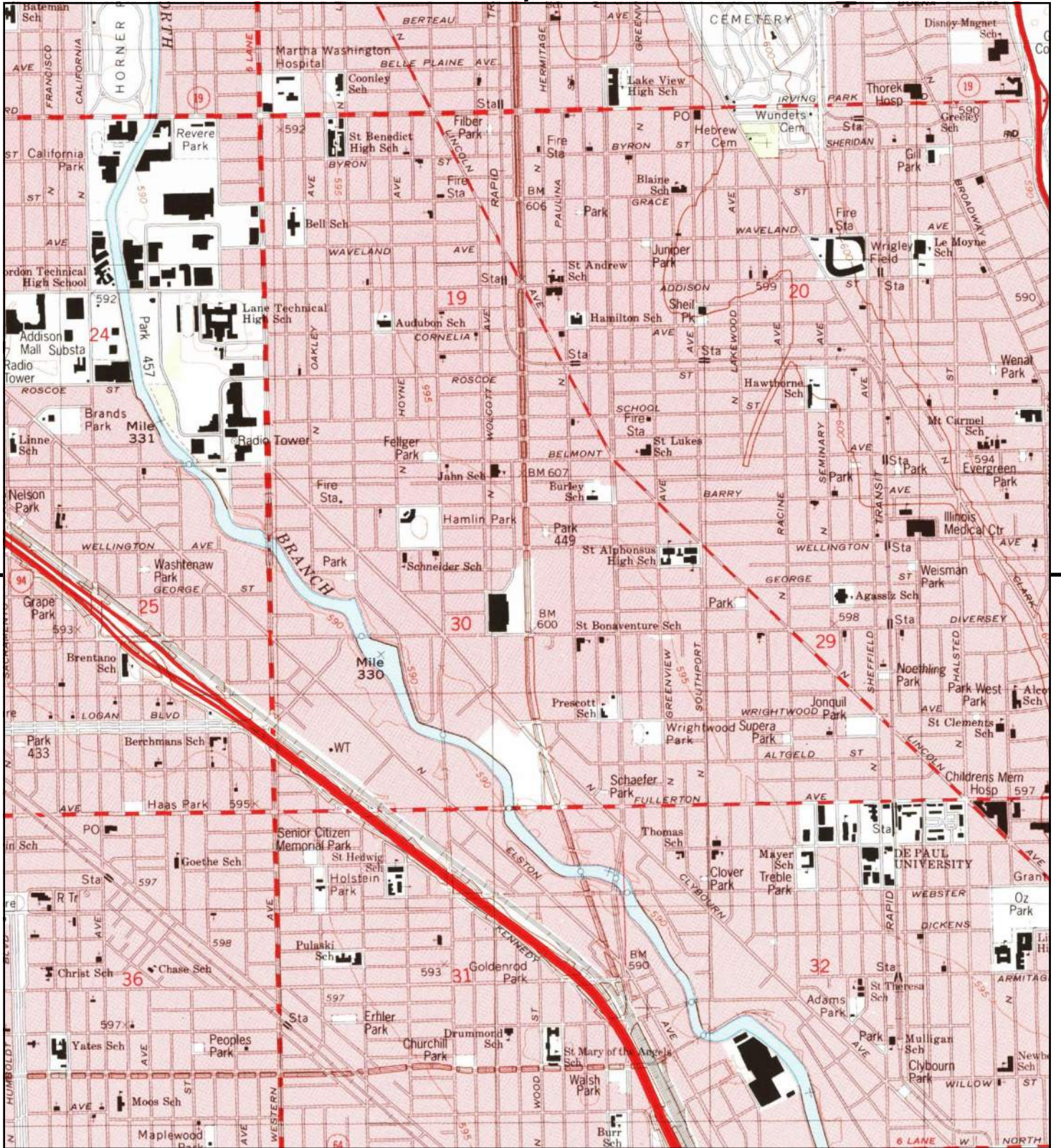
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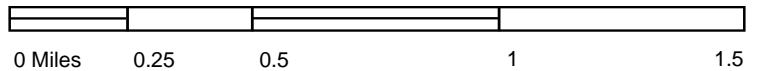
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 Chicago, IL 60657
 CLIENT: CDM Smith Inc.





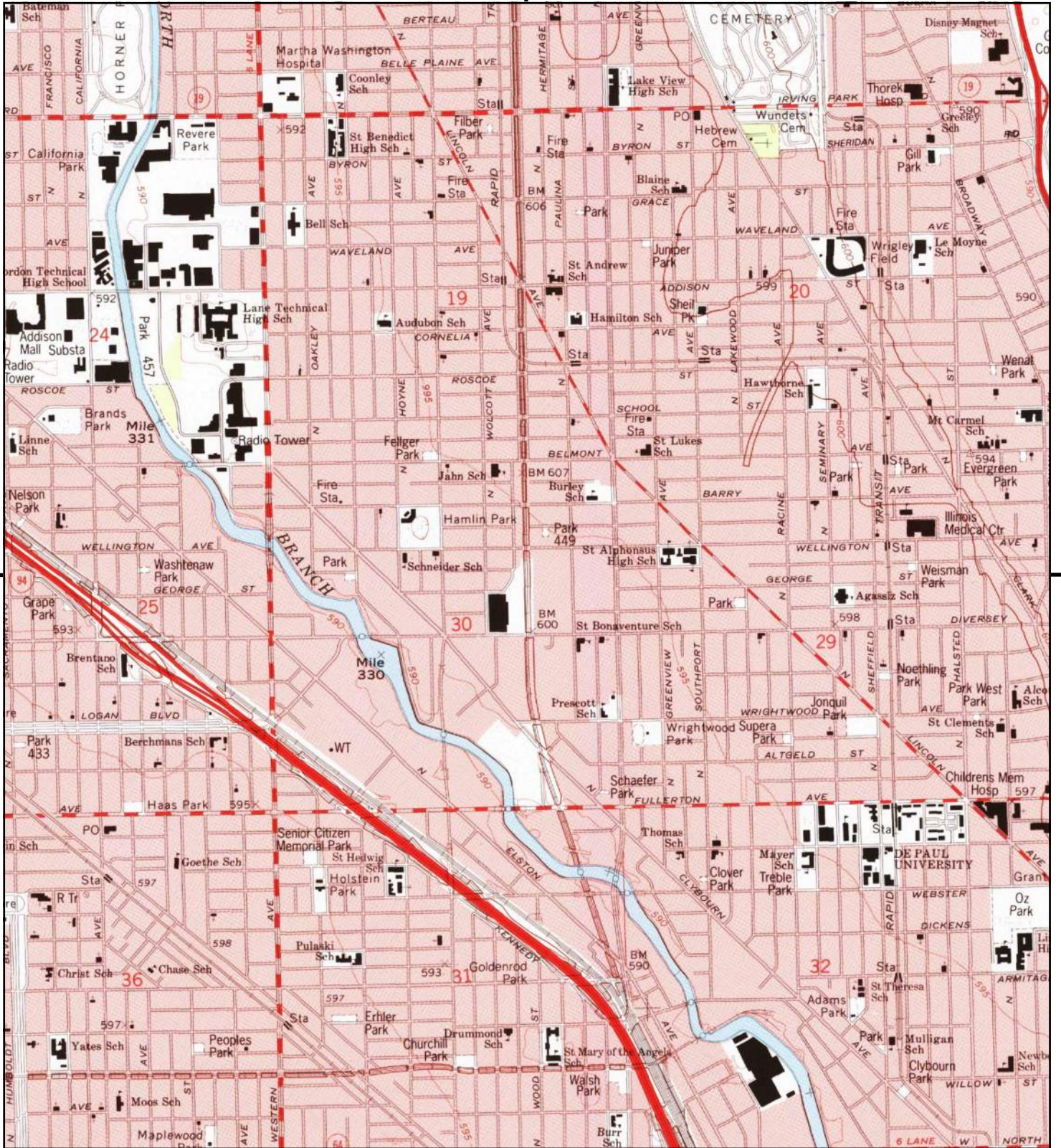
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 Chicago, IL 60657
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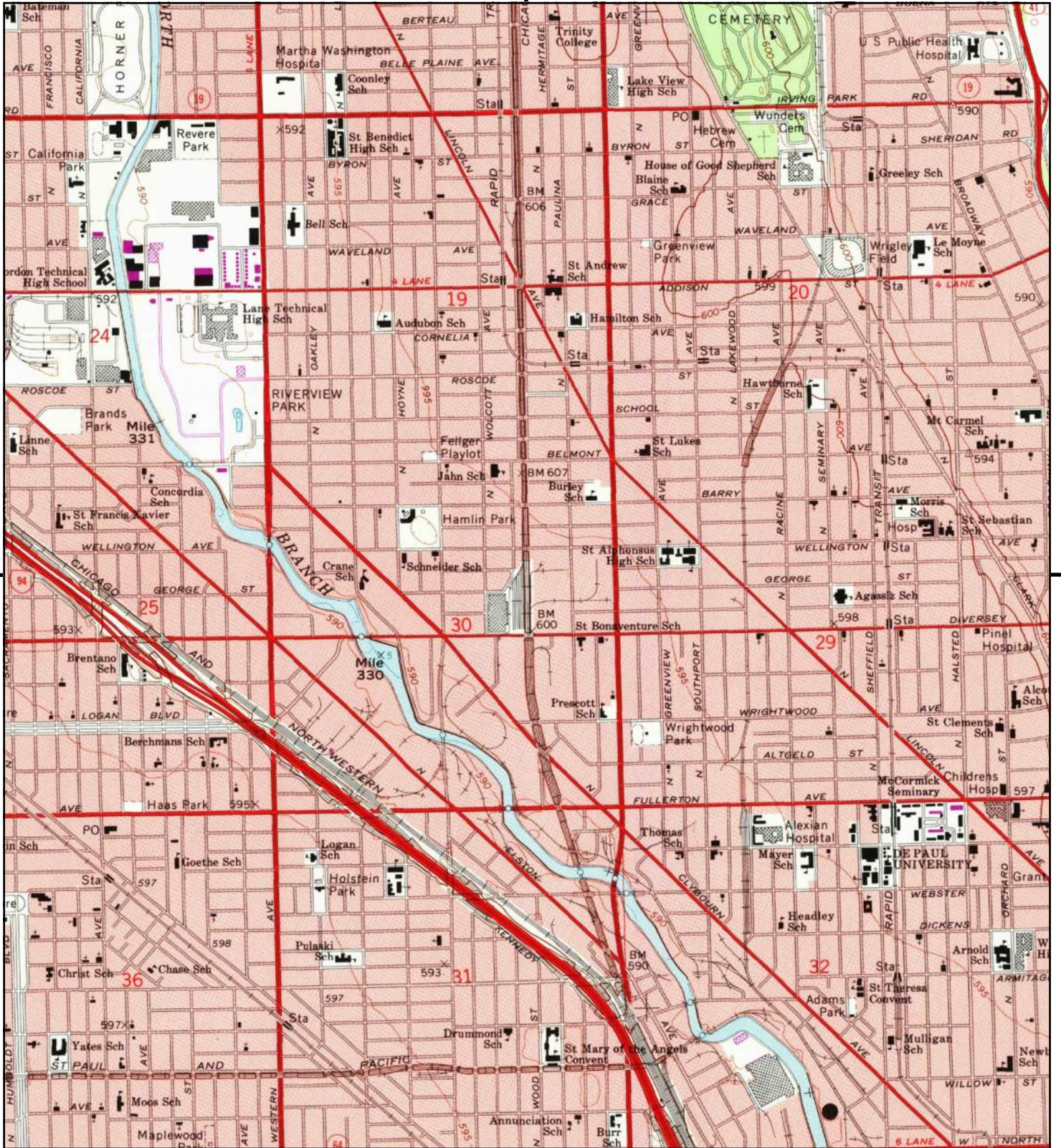
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 Chicago, IL 60657
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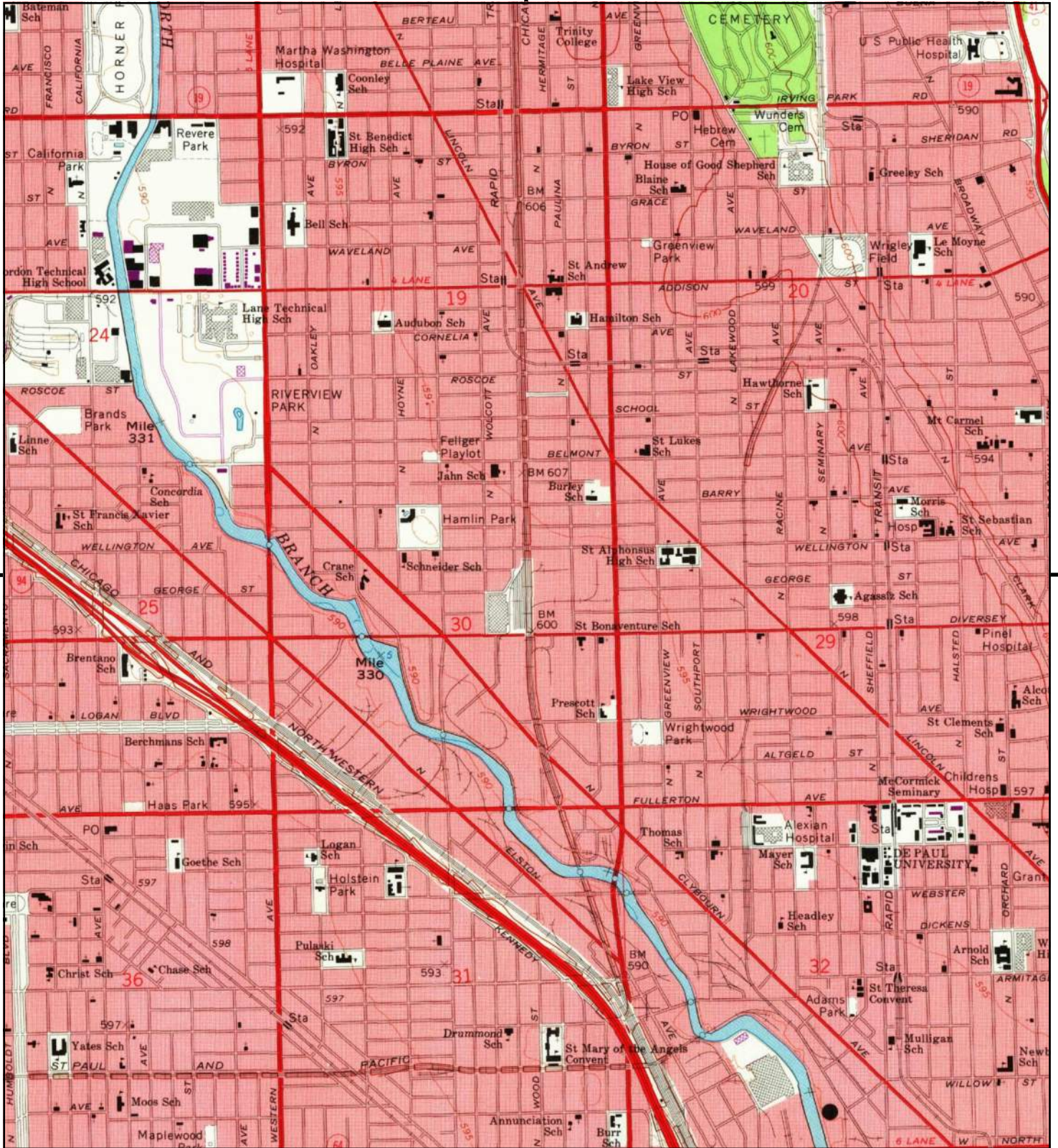
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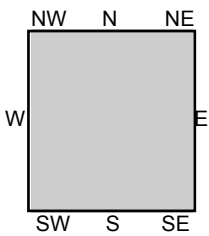
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 Chicago, IL 60657
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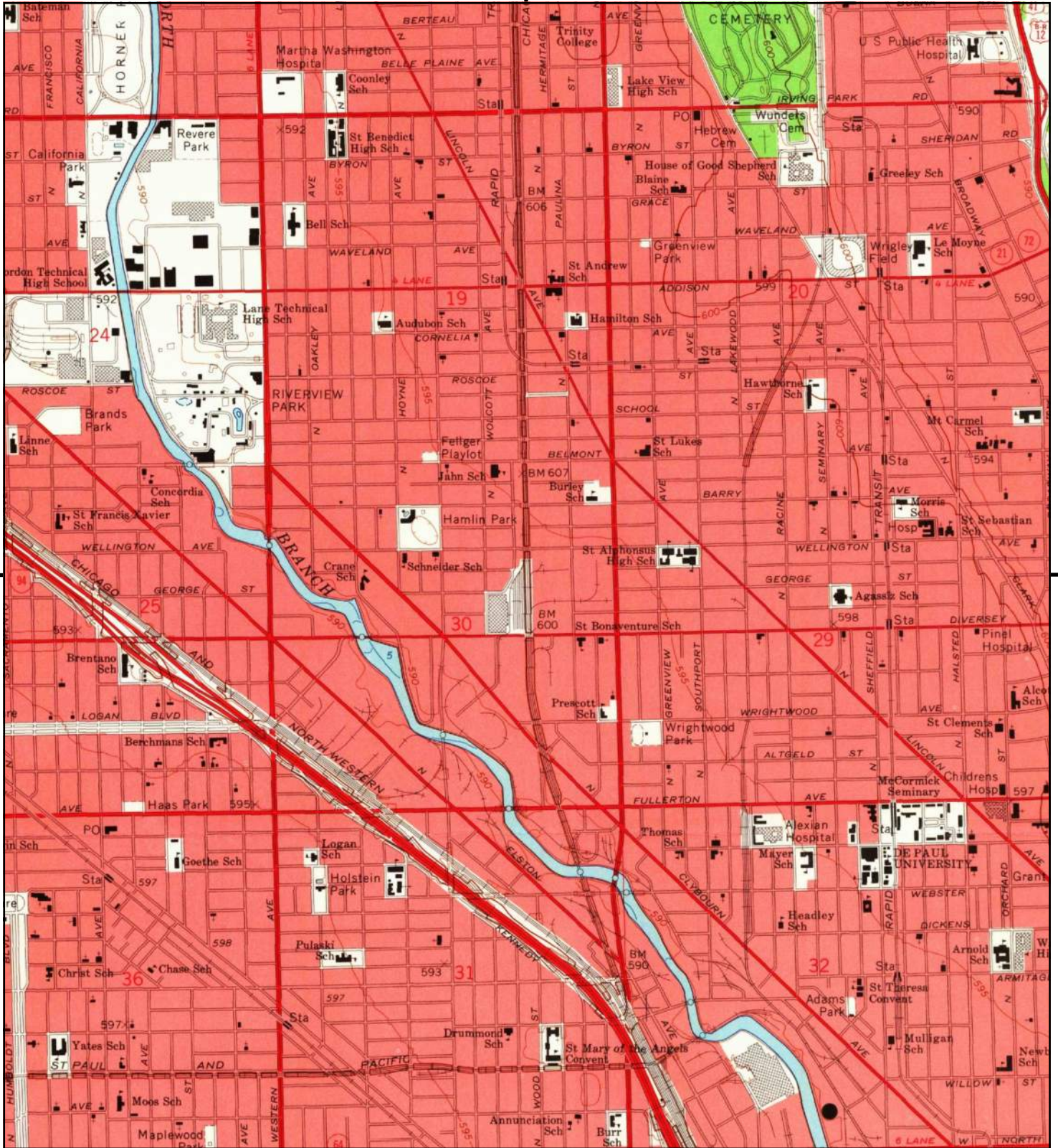
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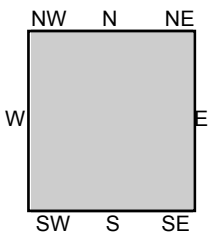
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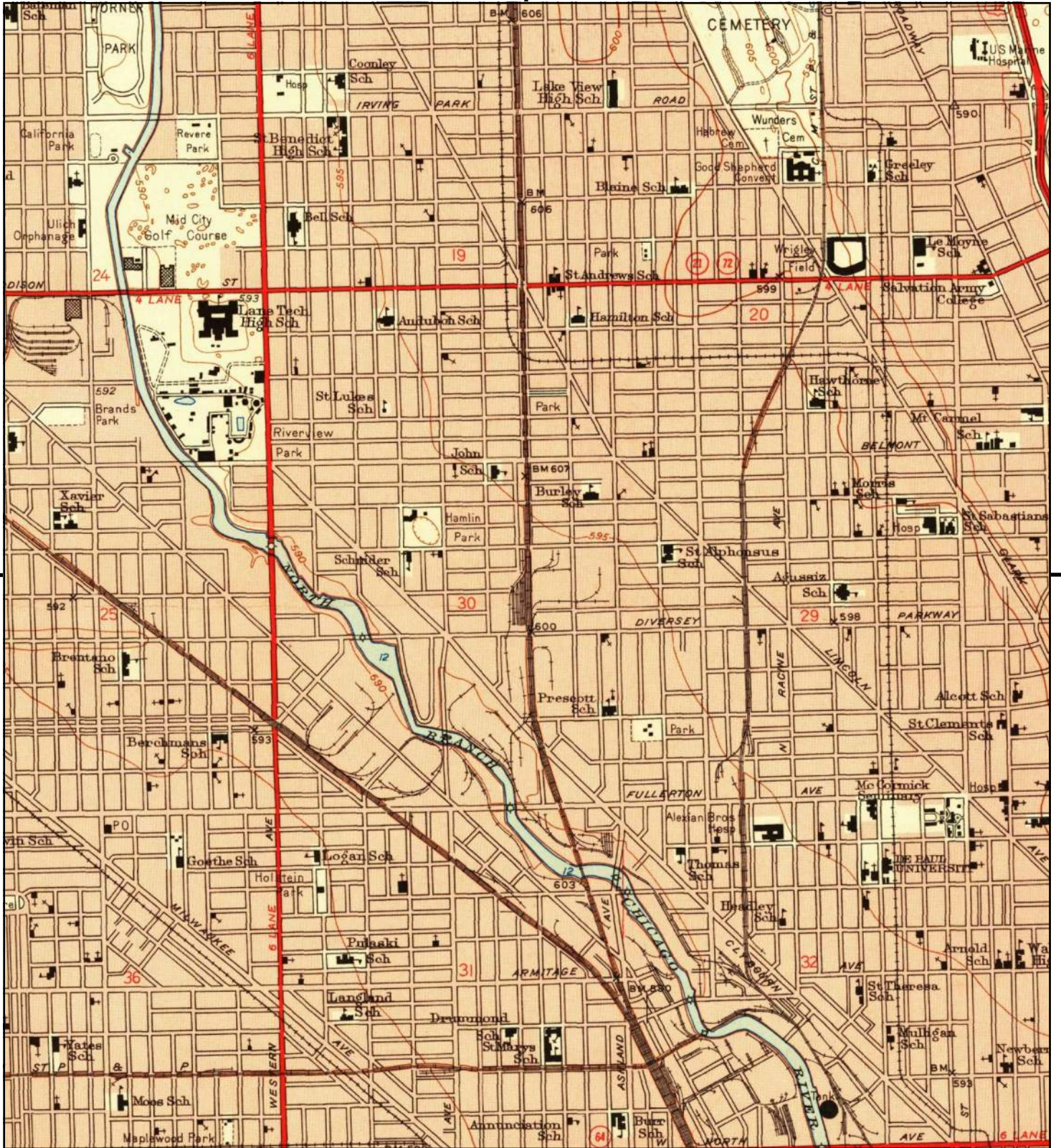
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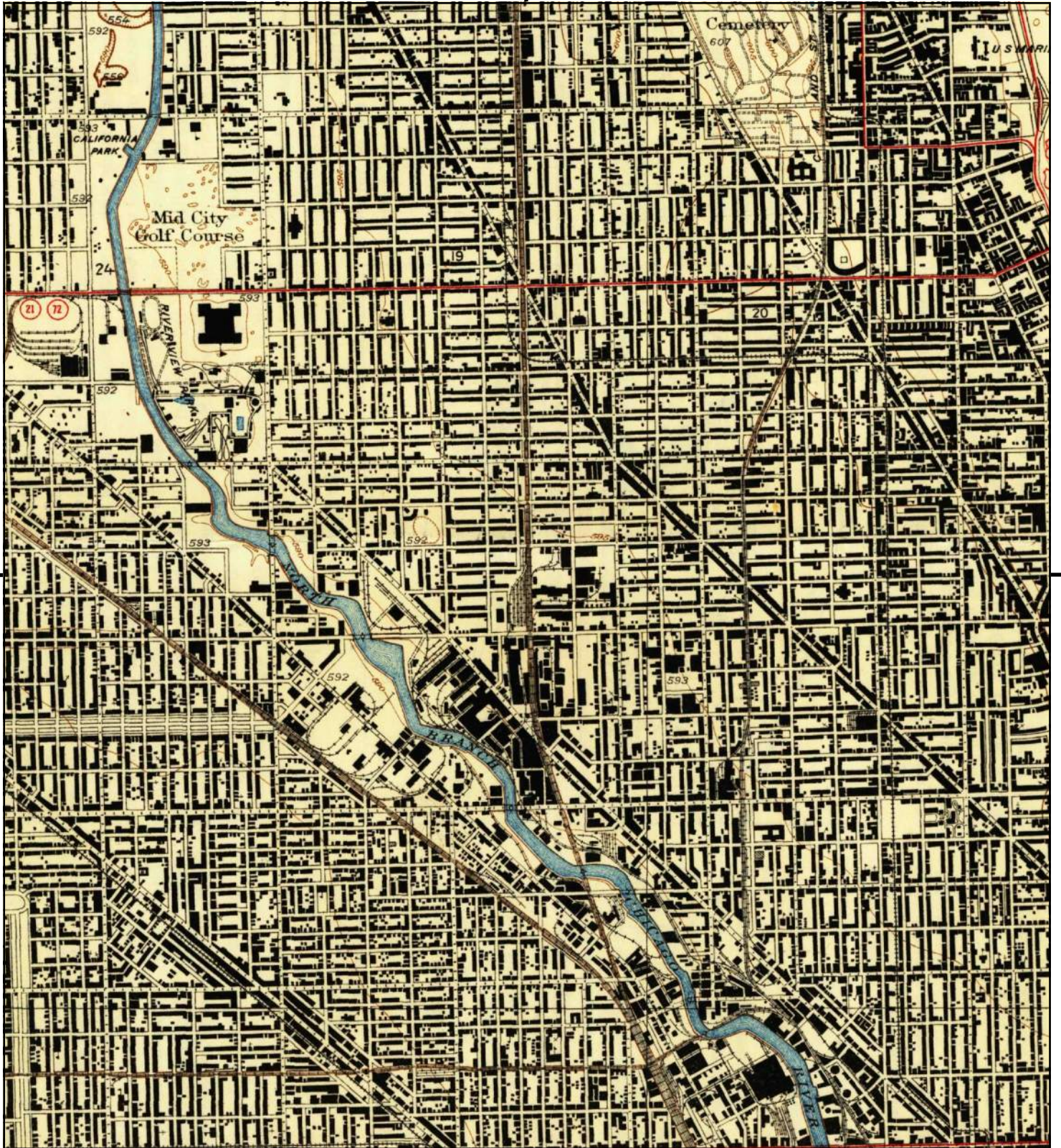
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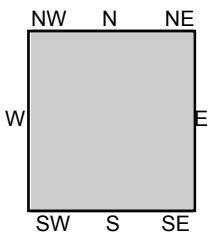
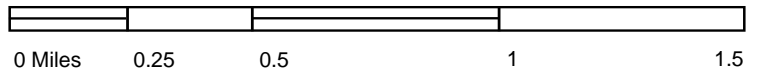
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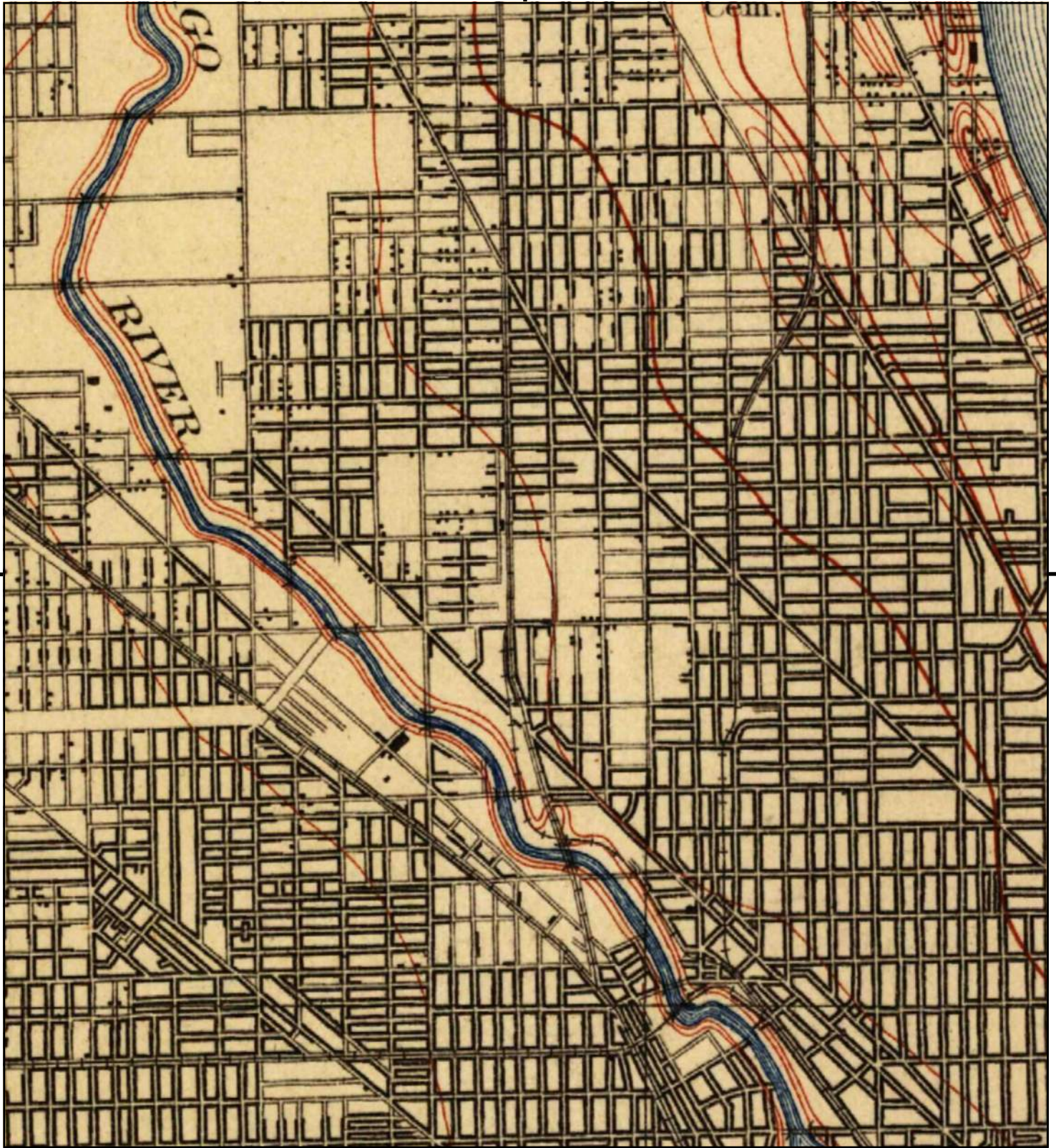
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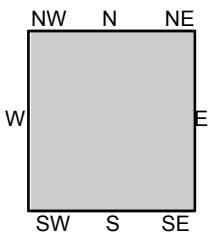
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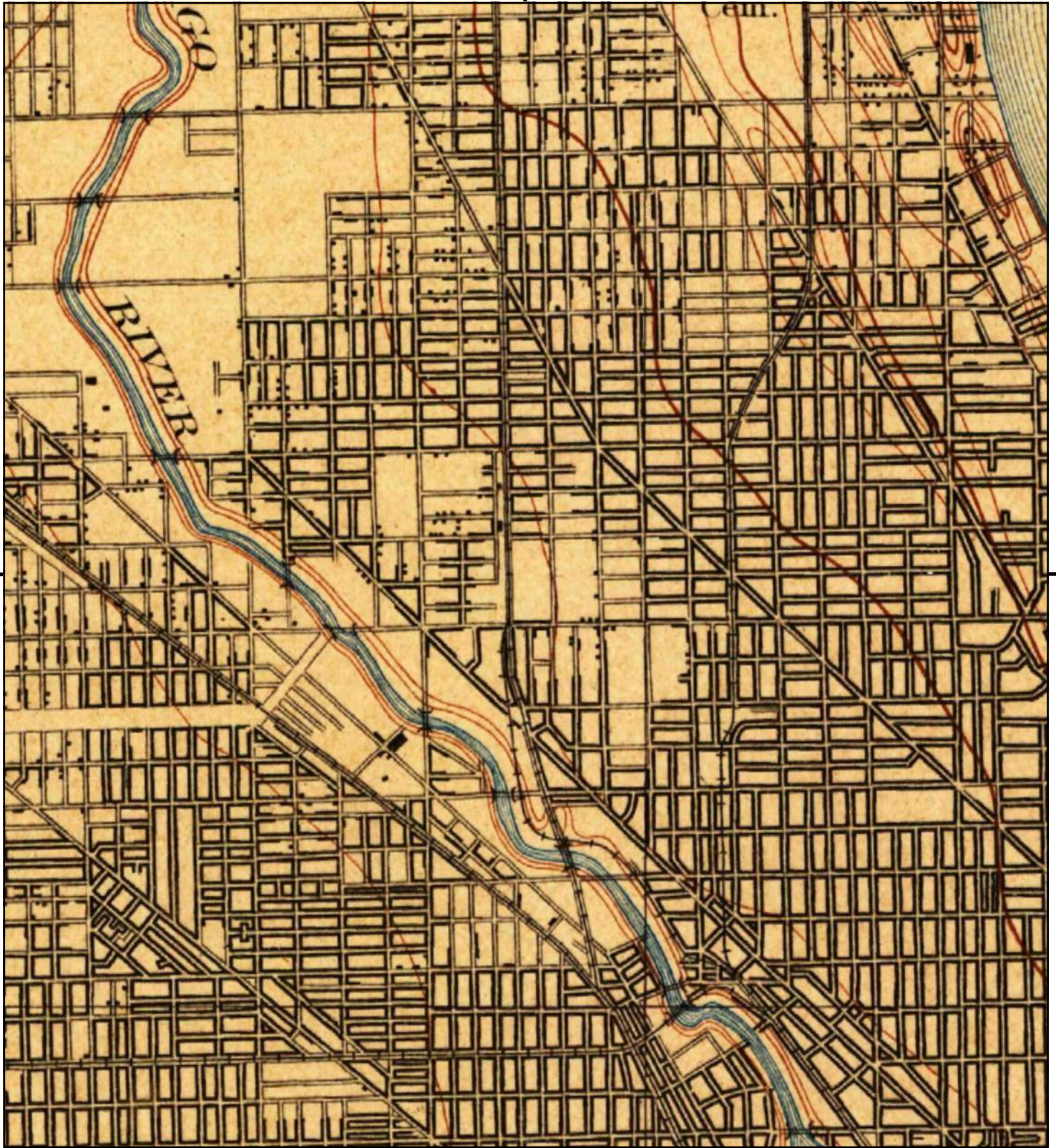
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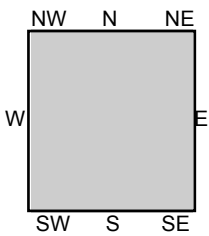
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 Chicago, IL 60657
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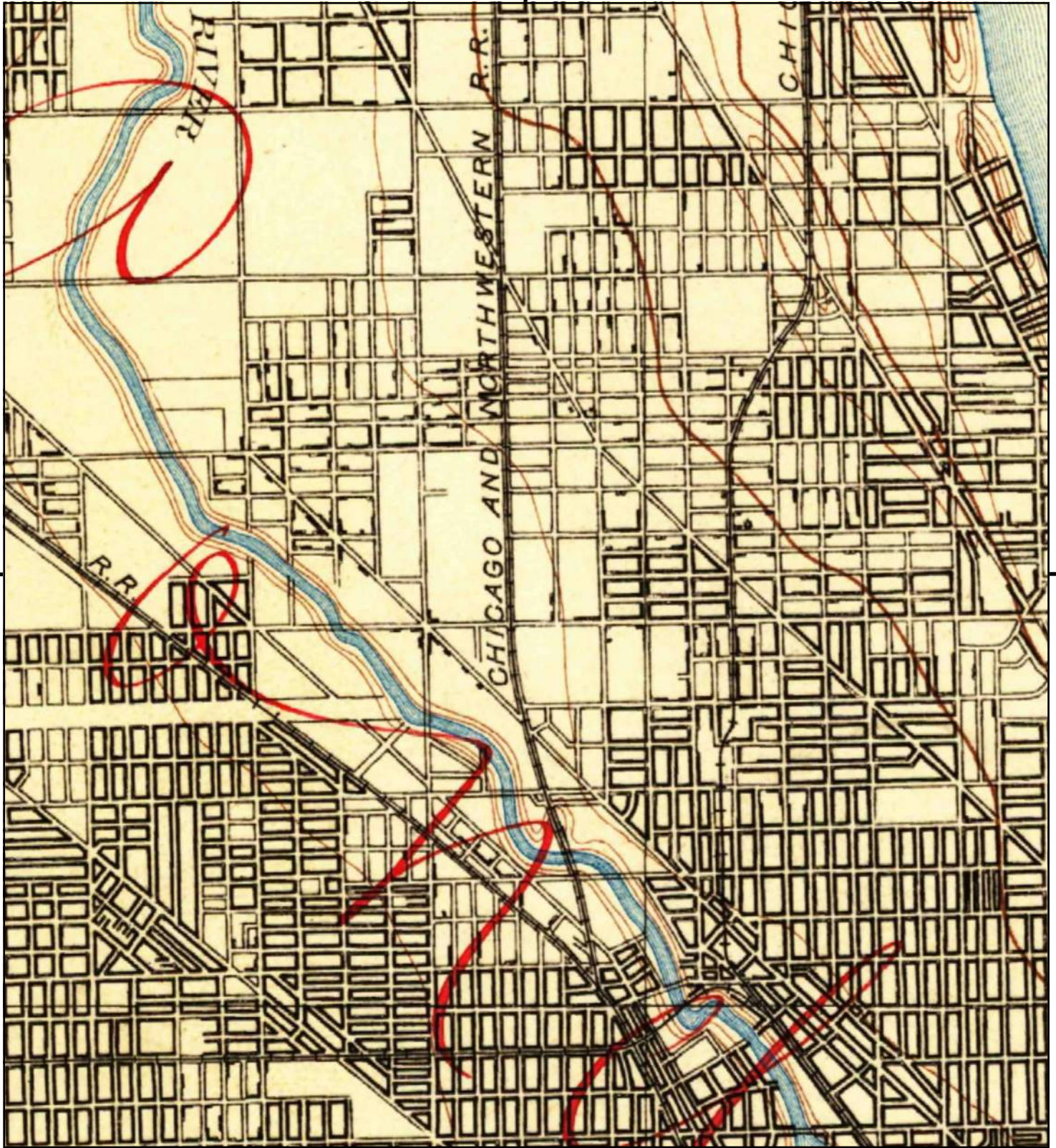
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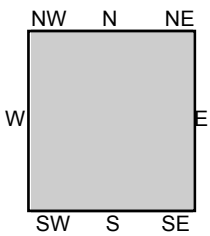
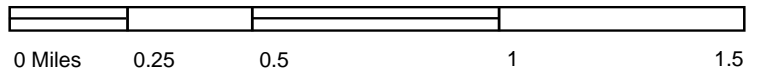
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 ADDRESS: NEPA-BRIDGES
 Chicago, IL 60657
 CLIENT: CDM Smith Inc.





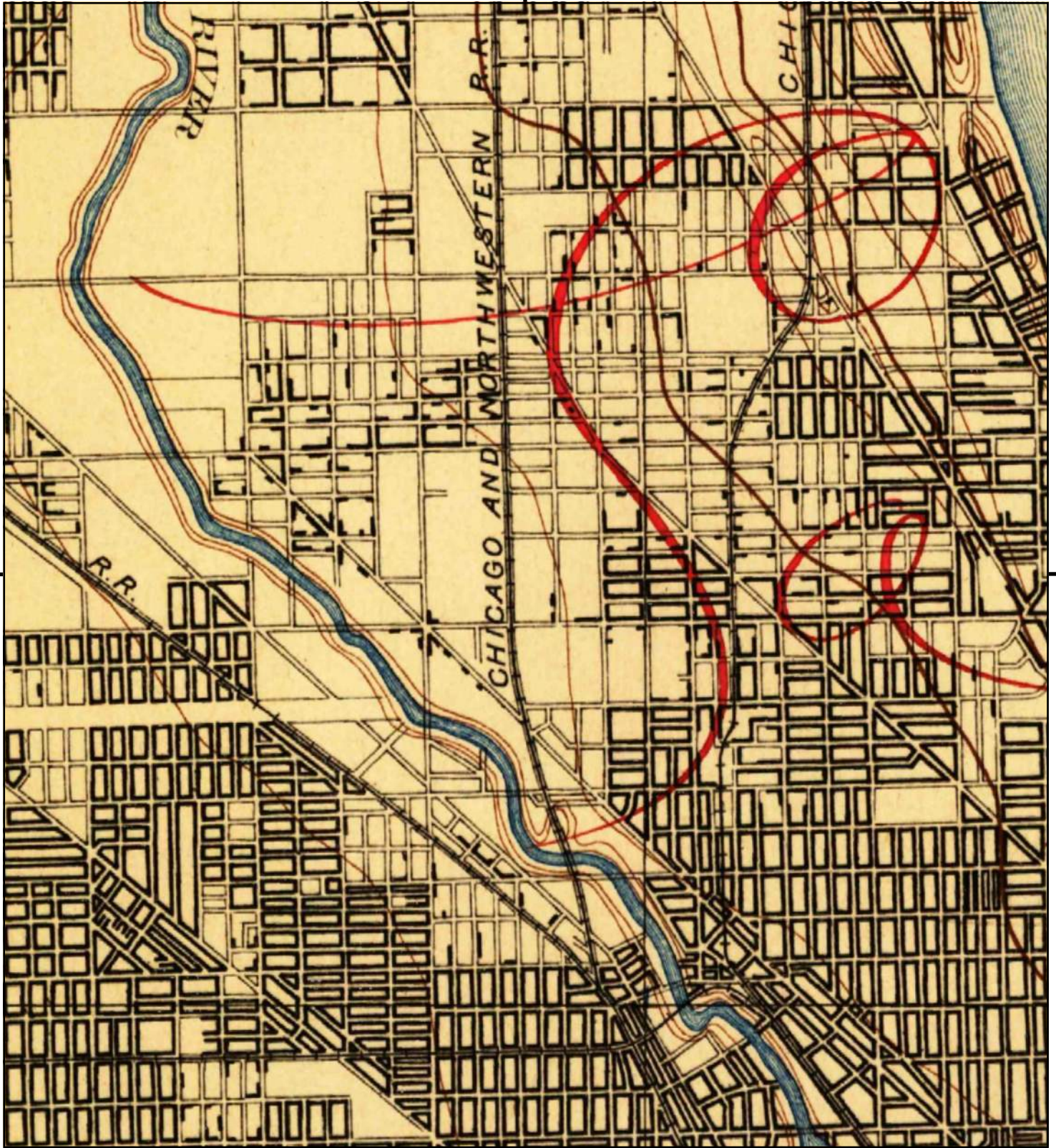
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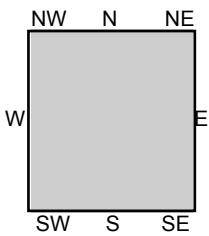
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 Chicago, IL 60657
 CLIENT: CDM Smith Inc.





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 ADDRESS: NEPA-BRIDGES
 Chicago, IL 60657
 CLIENT: CDM Smith Inc.



Appendix E

Natural Resources Technical Memorandum

Metra UP North Rebuild: Fullerton to Addison



Natural Resources Technical Memorandum

Metra project No. 4840
CDMS project No. 261845
Hey project No. 20-0319

February 24, 2022

Prepared for:



547 W. Jackson Blvd.
Chicago, IL 60661

Prepared by:



125 S. Wacker Dr.
Suite 700
Chicago, IL 60606

Hey and Associates, Inc.
Engineering, Ecology and Landscape Architecture

26575 W. Commerce Drive
Suite 601
Volo, IL 60073

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Section 1

Introduction

This technical memorandum has been prepared to provide information on the natural resources within the Project corridor. The Project corridor extends along the Union Pacific (UP) North line from just north of the Chicago River Bridge (UP Mile Post [MP] 3.27) north to approximately Grace Street (UP MP 5.22) in Chicago, Cook County, Illinois. The corridor includes two active rail lines on the eastern side, and space for and remnants of a third line on the west. This investigation included the full width of the railroad right-of-way including the embankments. In many cases, the right-of-way limits were not clear in the field. A best judgement was made in the field based on fence lines and similar features to discern the extent of the Project corridor. The study deliberately focused on areas within the right-of-way where improvements are planned. A general assessment of existing conditions immediately adjacent to the right-of-way was noted during field investigations.

Field work for this report was completed on June 21 and August 24, 2021. Desktop reviews of available data and online resources were completed in the two weeks prior to the June 21 field tour and supplemented during the time between the field inspections. Field inspections included visual and auditory observations of plant and animal species within the Project corridor. Field staff were always accompanied by METRA and CDM Smith staff.

Based on the assessment conducted, there is no suitable habitat for any of these federally listed species in or near the Project corridor. No critical habitat is designated within the project area. A determination of “no effect” can be recommended for federally listed species. Further information is provided in Section 6.1.

Coordination with the Illinois Department of Natural Resources (IDNR) indicated that the Illinois Natural Heritage Database contains no records of state-listed threatened or endangered species in the vicinity of the project area. Further information is provided in Section 6.2.

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Section 2

Project Overview

The Federal Transit Administration (FTA) and Metra are proposing to shift the track alignment and replace abutments, retaining walls and 11 bridges over roadways along approximately 1.9 miles of the UP North Line above-grade commuter rail corridor. This corridor is within railroad right-of-way from north of the North Branch Chicago River bridge (UP MP 3.27) to approximately Grace Street (UP MP 5.22) in Chicago, Illinois. The Project is located just south of the UP North Line Grace to Balmoral Project, which replaced 11 railroad bridges and rebuilt the existing Ravenswood train station. A Documented Categorical Exclusion (DCE) environmental document was completed and approved by Metra and the FTA for the Grace to Balmoral Project in June 2011; construction of the bridges is complete and station construction is ongoing. No permanent property acquisition or changes to the existing railroad right-of-way boundary are anticipated for this Project. The Project would include the following elements:

- Eleven (11) railroad bridges inclusive of the abutments would be replaced over the roadway from Fullerton Avenue to Cornelia Avenue along Metra's UP North Line.
- The roadway would be lowered by approximately 1 to 2 feet at Roscoe Street and Cornelia Avenue to maintain adequate vertical clearance for traffic underneath the bridges.
- Retaining walls would be replaced, and new retaining walls would be added from Fullerton Avenue to north of Lincoln Avenue/Addison Street along the Metra UP North Line. New retaining walls would be used to maintain the Project footprint within the existing right-of-way.
- A 12th existing steel railroad bridge at Lincoln Avenue/Addison Street on the UP North Line would be refurbished. Work would include lead abatement, prepping, and painting at the bridge.
- Track alignment would be shifted to the west throughout the Project corridor in order to align with the existing tracks at the Grace Street bridge north of the Project limits and the North Branch Chicago River bridge south of the Project limits. All track alignment adjustment would be located within the existing right-of-way and would continue to be refined in final design. The track height would be raised by up to 3 feet to maintain adequate vertical clearance under the bridges.
- Roadway reconstruction is anticipated at Cornelia Avenue and Roscoe Street and roadway resurfacing is anticipated at Clybourn Avenue and Fullerton Avenue. At these locations, all existing pavement and roadway markings would be replaced in accordance with CDOT rules and regulations. At other bridge replacement locations, roadway repaving and lane marking would occur if necessary following construction activities.
- Pedestrian improvements would include American with Disabilities Act (ADA) compliant sidewalks and restriped crosswalks where bridge underpasses are being reconstructed. No new sidewalks are proposed.

Some utility relocation and replacement would also be required to accommodate these infrastructure improvements. Existing utilities would be replaced or relocated at bridge structure replacement locations, along roadways that are to be lowered at Roscoe Street and Cornelia Avenue, at abutment replacement locations at Fullerton Avenue and Clybourn Avenue, and at some retaining wall locations.

This Project is located entirely within the City of Chicago. The Project begins proximate to the North Branch Chicago River immediately north of the Deering bridge (UP MP 3.27) and ends at Grace Street bridge (UP MP 5.22). **Figure 1** provides a Project location map.

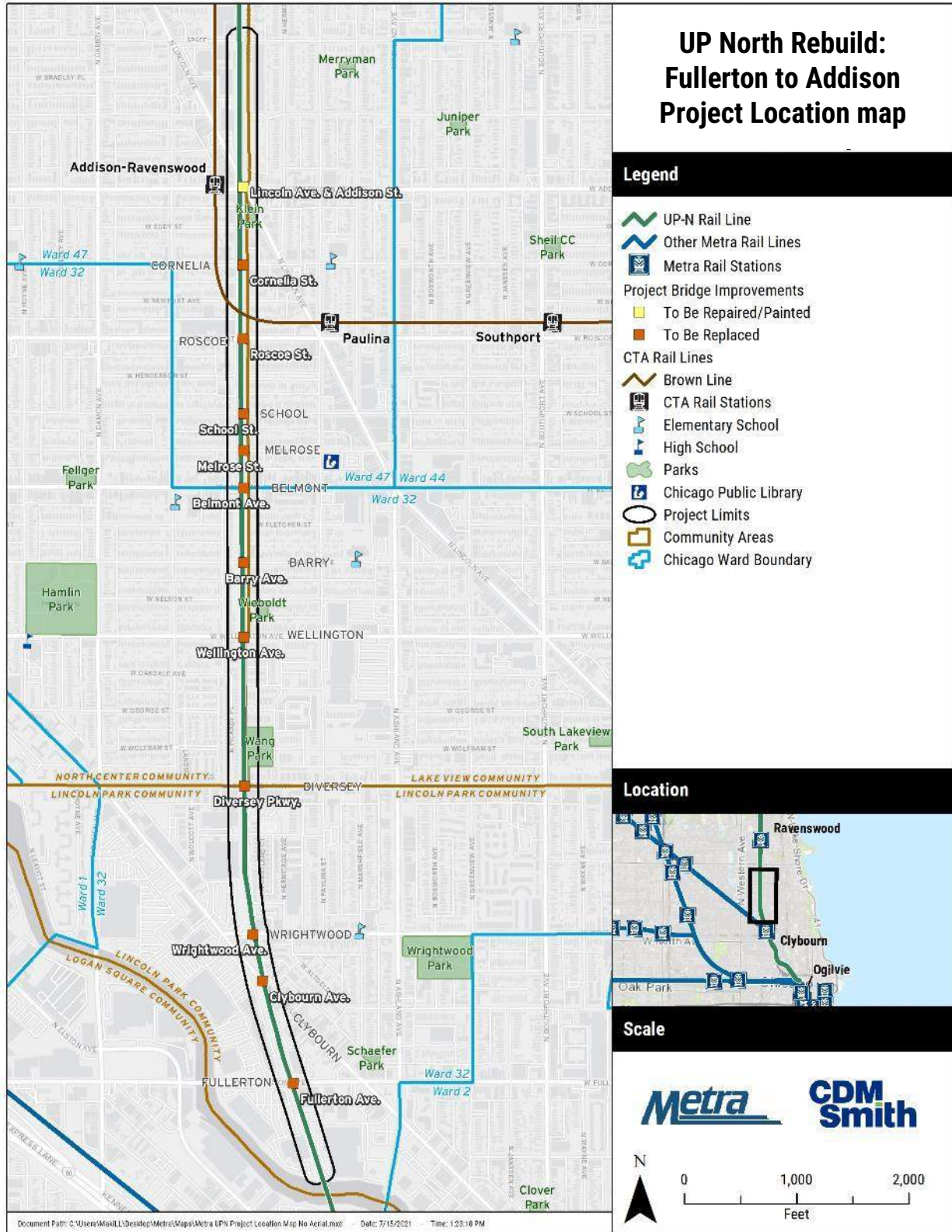


Figure 1: Project Location Map

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Section 3

Methodology

This investigation included the full width of the railroad right-of-way including the embankments. In many cases, the right-of-way limits were not clear in the field. A best judgement was made in the field based on fence lines and similar features to discern the extent of the Project corridor. The study deliberately focused on areas within the right-of-way where improvements are planned. A general assessment of existing conditions immediately adjacent to the right-of-way was noted during field investigations. The length of the study area was as described in the Project overview and did not extend farther than the area just north of the Chicago River on the south, and the Grace Avenue bridge on the north.

Field work for this memorandum was completed on June 21 and August 24, 2021. Desktop reviews of available data and online resources were completed in the two weeks prior to the June 21 field tour and supplemented during the time between the field inspections. Desktop reviews consisted of retrieval and review of available published resources including current and historic aerial photographs, soil type mapping, National Wetlands Inventory mapping, and data layers on the Cook County online parcel viewer. The resources reviewed for protected lands, endangered and threatened species, and wetlands are documented in those respective sections below.

The field reconnaissance on June 21, 2021, was completed in part on foot, and in part in vehicles. It took place from approximately 1pm until 4pm CDT that day and conditions were warm and sunny. It included a visual review from the east and west sides of the Project area by the Project team. Digital photographs were taken at selected locations that provided typical views or unique features. One ecologist with 35 years of experience in the Chicago region focused on observations of vegetation, habitat, and any wildlife. The Chicago region, as defined by Swink and Wilhelm's *Plants of the Chicago Region*, 1994, consists of the 22 counties at the southern tip of Lake Michigan and surrounding downtown Chicago, Illinois.

The field inspection on August 24, 2021, consisted of walking the railroad tracks for the entire project area. Conditions were sunny, hot, and humid (ambient temperatures in the upper 80s and lower 90s Fahrenheit) and took place from approximately 9am until 1pm CDT. One ecologist with 35 years of experience in the Chicago region focused on observations of vegetation, habitat, and any wildlife. Field staff were always accompanied by a railroad flagger and CDM Smith staff.

The field technique consisted of time meander and visual encounter searches. This involved walking along the corridor and noting all species of plants within the study corridor, moving on from each area when no new species were observed after a 10 to 15 minute time period for a given area of habitat. Similarly, wildlife observations were recorded as species were encountered or observed while walking the study corridor. No quantitative sampling was used and no transects were established. The goal was to generally characterize the existing vegetation and wildlife usage and note areas or resources of particular concern (i.e., protected species or habitat). All of the resources described herein represent existing conditions.

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Section 4

Existing Vegetation

The existing railroad tracks are elevated above the adjacent landscape through this corridor. There is a varying width band of vegetation along both sides of the right-of-way depending on the slope and presence of retaining walls. The elevated railroad corridor afforded a good vantage point to observe any adjacent habitat features. The area adjacent to the railroad right-of-way is entirely urban, dominated by residential neighborhoods, industrial, and commercial land uses consisting of paved streets, alleys, parking lots, and sidewalks, buildings and other structures, and small areas of manicured turf and formal landscaping. In many places, large warehouses and residential dwellings immediately abut the railroad right-of-way.

This section is describing the existing vegetation within the defined railroad right-of-way and immediately adjacent to it. In this context, weedy is used to describe fast-growing species that spread or reproduce prolifically. They are often non-native species and ecologically undesirable in a given habitat.

The vast majority of existing vegetation is composed of common, often non-native urban weeds. Not all non-native species are invasive, but there are some occurrences of species known to be invasive in the Chicago region. No native plant communities were observed, though there may be occasional native species scattered throughout.

The dominant tree species across the entire corridor is tree of heaven (*Ailanthus altissima*) (also known as varnish tree), which is non-native and very weedy. It is considered invasive by many due to its prolific reproduction from seeds. Most of the woody growth in the corridor, consists of young trees and shrubs. There were very few mature canopy trees observed (>25) in the corridor. The woody vegetation is dominated by young, fast-growing silver maple, green ash, box elder, and tree of heaven.

The existing herbaceous vegetation present is dominated by species that can survive in the harsh urban conditions along a railroad. All herbaceous plant species noted during the June and August 2021 field inspections along with their floristic quality data are listed in **Table 4-1** below. The Floristic Quality Assessment (FQA) system as originally published in Swink and Wilhelm's *Plants of the Chicago Region*, 1994, and updated in Wilhelm and Rericha's *Flora of the Chicago Region*, 2017 assigns a rating to plant species that reflects the fundamental conservatism that the species exhibits for natural habitats. A native species that exhibits specific adaptations to a narrow spectrum of the environment is given a high rating. Conversely, a ubiquitous species that exhibits adaptations to a broad spectrum of environmental variables is given a low rating. Using this method, a Floristic Quality Index (FQI) is derived for a given area. The FQI is an indication of native vegetative quality for an area: generally, 1-19 indicates low vegetative quality, 20-35 indicates high vegetative quality and above 35 indicates "Natural Area" quality.

The inventory presented in **Table 4-1** was derived using the U.S. Army Corps of Engineers' Chicago FQA Calculator Version 11, November 2017. The column labeled "Species Name" follows the taxonomic nomenclature of the National Wetland Plant List (NWPL). Species not found in the NWPL follow the taxonomic nomenclature of *Flora of the Chicago Region: A Floristic and Ecological Synthesis* (Wilhelm and Rericha 2017).

The columns labeled 'Habit', 'Duration' and 'Nativity' follow information provided in the National Wetland Plant List (Lichvar 2013, Kartesz, J.T. 2013, Swink and Wilhelm 1994).

In **Table 4-1** each species is provided with its coefficient of conservatism (C value) (0 = weedy, 10 = conservative), the plant habit (grass, forb, sedge, etc.), duration (annual, perennial, biennial), and nativity. Non-natives include those species that were anthropogenically introduced to the region since European settlement and are therefore not integral to any pre-settlement plant community.

The information above the species list provides an analysis of the vegetative quality of the site. It shows the total number of species present (species richness), the mean coefficient of conservatism (Mean C), the floristic quality assessment index (FQAI); calculated separately for native species only and including the non-native species. The Mean C datum indicates the average coefficient of conservatism. The FQAI is derived by multiplying the Mean C by the square root of the number of species. If the FQAI of a particular area register in the middle 30s or higher, one can be relatively certain that there is sufficient native character to be of environmental importance in terms of a regional natural area.

Invasive species observed in the corridor include tree of heaven, Japanese knotweed, Asian bittersweet, and Siberian elm. These are all known to have significant adverse impacts on natural areas and native communities. They are very aggressive and may take over the corridor if left unchecked. Other species on this list are also considered aggressive or weedy but these four species are the most invasive.

Table 4-1: Plant species recorded in the railroad right-of-way with floristic quality information.

SITE: Union Pacific North Line, Fullerton to Grace
 LOCALE: Chicago, Cook County, Illinois
 BY: J. Mengler (Hey and Associates)
 DATES: 8/24/2021 & 6/21/2021

Conservatism-Based Metrics		Additional Metrics	
Mean C (native Species)	1.96	Species Richness (all)	46
Mean C (all Species)	1.11	Species Richness (native)	26
Mean C (native Trees)	2.83	Percent Non-native	0.43
Mean C (native Shrubs)	0.00	Percent Native perennial	0.48
Mean C (native herbaceous)	1.09	Percent Native Annual	0.07
FQAI (native Species)	10.00	% Annual	0.13
FQAI (all Species)	7.52	% Perennial	0.78

Species Name	Common Name	C Value	Habit	Duration	Nativity
<i>Acer negundo</i>	Box Elder	0	Tree	Perennial	Native
<i>Acer platanoides</i>	Norway Maple	0	Tree	Perennial	Non-native
<i>Acer saccharinum</i>	Silver Maple	1	Tree	Perennial	Native
<i>Ailanthus altissima</i>	Tree-of-Heaven	0	Tree	Perennial	Non-native
<i>Alnus glutinosa</i>	European Alder	0	Tree	Perennial	Non-native
<i>Ambrosia artemisiifolia</i>	Annual Ragweed	0	Forb	Annual	Native
<i>Apocynum cannabinum</i>	Indian-Hemp	2	Forb	Perennial	Native
<i>Artemisia vulgaris</i>	Common Mugwort	0	Forb	Perennial	Non-native
<i>Asclepias syriaca</i>	Common Milkweed	0	Forb	Perennial	Native
<i>Catalpa speciosa</i>	Northern Catalpa	0	Tree	Perennial	Non-native
<i>Celastrus orbiculatus</i>	Asian Bittersweet	0	Vine	Perennial	Non-native
<i>Celtis occidentalis</i>	Common Hackberry	2	Tree	Perennial	Native
<i>Chenopodium album</i>	Lamb's-Quarters	0	Forb	Annual	Non-native
<i>Daucus carota</i>	Queen Anne's Lace	0	Forb	Biennial	Non-native
<i>Eragrostis pectinacea</i>	Purple Love Grass	0	Grass	Annual	Native
<i>Erigeron canadensis</i>	Canadian Horseweed	0	Forb	Annual	Native
<i>Eupatorium altissimum</i>	Tall Boneset	0	Forb	Perennial	Native
<i>Euthamia graminifolia</i>	Flat-Top Goldentop	4	Forb	Perennial	Native
<i>Fraxinus pennsylvanica</i>	Green Ash	4	Tree	Perennial	Native

Species Name	Common Name	C Value	Habit	Duration	Nativity
<i>Gleditsia triacanthos</i>	Honey-Locust	1	Tree	Perennial	Native
<i>Juglans nigra</i>	Black Walnut	3	Tree	Perennial	Native
<i>Linaria vulgaris</i>	Butter-and-Eggs	0	Forb	Perennial	Non-native
<i>Medicago lupulina</i>	Black Medick	0	Forb	Annual	Non-native
<i>Melilotus albus</i>	White Sweet-Clover	0	Forb	Biennial	Non-native
<i>Morus alba</i>	White Mulberry	0	Tree	Perennial	Non-native
<i>Oenothera biennis</i>	Evening Primrose	0	Forb	Biennial	Native
<i>Parthenocissus quinquefolia</i>	Virginia-Creeper	4	Vine	Perennial	Native
<i>Phytolacca americana</i>	American Pokeweed	0	Forb	Perennial	Native
<i>Picea abies</i>	Norway Spruce	0	Tree	Perennial	Non-native
<i>Pinus sylvestris</i>	Scots Pine	0	Tree	Perennial	Non-native
<i>Platanus occidentalis</i>	American Sycamore	5	Tree	Perennial	Native
<i>Poa pratensis</i>	Kentucky Blue Grass	0	Grass	Perennial	Non-native
<i>Populus deltoides</i>	Eastern Cottonwood	0	Tree	Perennial	Native
<i>Prunus serotina</i>	Black Cherry	0	Shrub	Perennial	Native
<i>Quercus macrocarpa</i>	Burr Oak	5	Tree	Perennial	Native
<i>Quercus rubra</i>	Northern Red Oak	5	Tree	Perennial	Native
<i>Reynoutria japonica</i>	Japanese-Knotweed	0	Shrub	Perennial	Non-native
<i>Schizachyrium scoparium</i>	Little Bluestem	5	Grass	Perennial	Native
<i>Setaria pumila</i>	Yellow Foxtail	0	Grass	Annual	Non-native
<i>Solanum dulcamara</i>	Climbing Nightshade	0	Vine	Perennial	Non-native
<i>Solidago altissima</i>	Tall Goldenrod	1	Forb	Perennial	Native
<i>Tilia americana</i>	American Basswood	5	Tree	Perennial	Native
<i>Ulmus americana</i>	American Elm	3	Tree	Perennial	Native
<i>Ulmus pumila</i>	Siberian Elm	0	Tree	Perennial	Non-native
<i>Verbascum thapsus</i>	Woolly Mullein	0	Forb	Biennial	Non-native
<i>Vitis riparia</i>	River-Bank Grape	1	Vine	Perennial	Native

This analysis shows that 26 of the 46 species recorded in this cursory inventory are non-native species. Most species have a C value (coefficient of conservatism) of 0 or 1 indicating they are weedy ubiquitous species that are not indicative of any native habitat type.

In a few reaches of the Project corridor, the neighboring community had planted gardens below the embankment or retaining wall. While these gardens may not be important ecologically, they are of value to the local community and the people who maintain them. Species planted in these garden areas were not included in the inventory presented above.

Overall, the vegetation is predominantly invasive, non-native, and typical of unmaintained urban areas and provides minimal ecological value.

Section 5

Wetlands

The U.S. Fish and Wildlife Service's (USFWS) National Wetlands Inventory (NWI) map shows no waters or wetlands within the Project corridor. However, the North Branch Chicago River is shown as a Riverine Lower Perennial Unconsolidated Bottom Permanently Flooded (R2UBH) map unit adjacent to, but south of, the project area. **Figure 2** depicts the NWI map. In addition, the U.S. Department of Agriculture Natural Resources Conservation Service's (NRCS) soil survey maps depict the entire project area as 533 Urban Land with no hydric soils. **Figure 3** depicts the NRCS soil survey map. This is due to the fact that this part of the city was developed prior to the soils and wetland mapping efforts by the federal government in this area. Therefore, any wetlands that may have existed historically were gone prior to any wetland regulations or comprehensive mapping.

This mapping information coupled with our field reconnaissance on June 21 and August 24, 2021, leads to the conclusion that there are no existing wetlands within the project area. No areas with a prevalence of hydrophytic vegetation were observed and there were no indicators of wetland hydrology present.

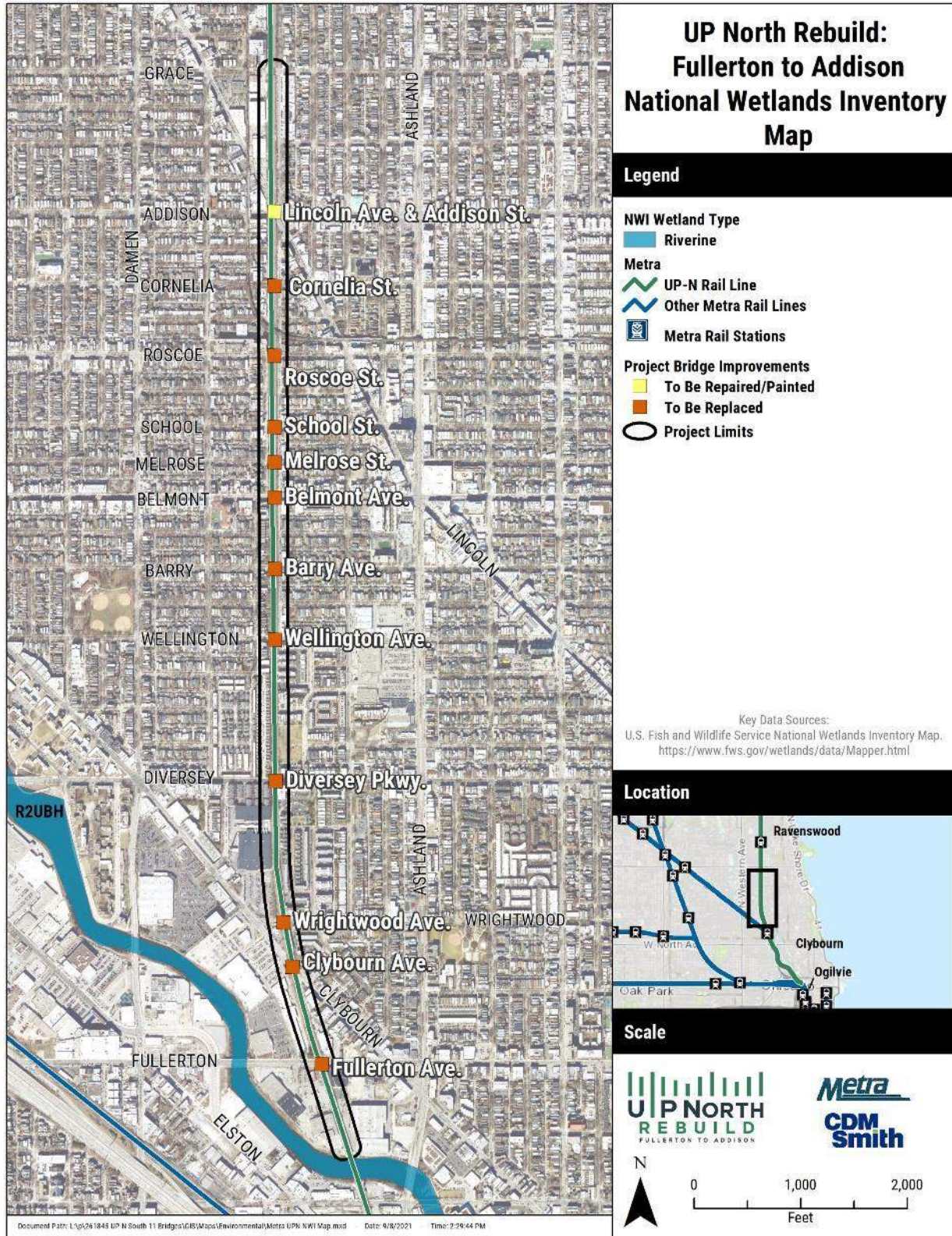


Figure 2: U.S. Fish and Wildlife Service National Wetlands Inventory Map

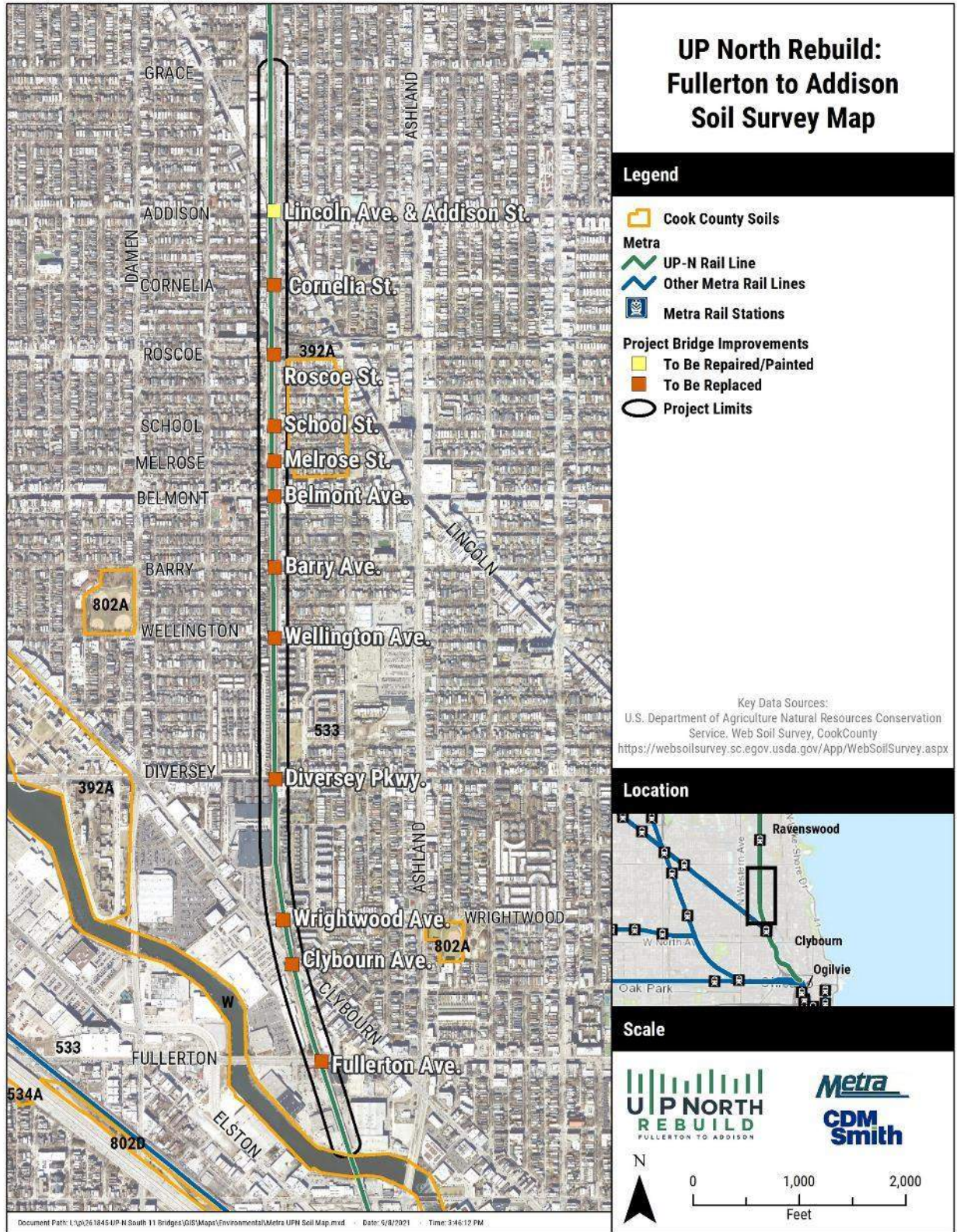


Figure 3: Soil Survey Map

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Section 6

Endangered and Threatened Species

6.1 Federally Listed Threatened and Endangered Species

The USFWS online Information for Planning and Consultation (IPAC) system provided a list of eight (8) federally listed species potentially in the project area (IPAC accessed on July 26, 2021). These species are listed in **Table 6-1**. A description of each species and its essential habitat requirements are provided below. The IPAC report from the Chicago Ecological Services Field Office is provided in Appendix A.

Table 6-1 Federally Listed Species Potentially in Project Area from the IPAC* Database.

Species	Common Name	Status
<i>Calidris cantus rufa</i>	Red Knot	Threatened
<i>Charadrius medodus</i>	Piping Plover	Endangered
<i>Dalea foliosa</i>	Leafy Prairie Clover	Endangered
<i>Lespedeza leptostachya</i>	Prairie Bush Clover	Threatened
<i>Myotis septentrionalis</i>	Northern Long-eared Bat	Threatened
<i>Platanthera leucophaea</i>	Eastern Prairie Fringed Orchid	Threatened
<i>Sistrurus catenatus</i>	Eastern Massasauga Rattlesnake	Threatened
<i>Somatochlora hineana</i>	Hine's Emerald Dragonfly	Endangered

*IPAC The USFWS online Information for Planning and Consultation geographic database accessed at <https://ecos.fws.gov/ipac/location/index> on July 26, 2021.

Northern Long-Eared Bat (*Myotis septentrionalis*)

The northern long-eared bat is a federally listed threatened bat species that hibernates in caves and mines, and swarms in surrounding wooded areas in autumn. It commonly roosts and forages in upland forests and woods during the summer. There are no caves or mines in the project area and therefore no hibernaculum habitat available. The USFWS Chicago Ecological Services Field Office and IDNR have indicated they are not aware of any known summer maternity roost trees in the project area. Based on this information and the state-listed species information provided below, it is not anticipated that any tree clearing restrictions or bridge inspections would be required.

Piping Plover (*Charadrius melodus*)

The piping plover is a federally listed endangered bird that uses wide, open, sandy beaches with very little grass or other vegetation for nesting. In Cook County, Illinois, it is restricted to the shores of Lake Michigan and no suitable habitat is present at or near the Project. Critical habitat has been designated by USFWS for this species, but it is entirely along the coast of Lake Michigan in Illinois and is not located within or near the project area.

Red Knot (*Calidris canutus rufa*)

The red knot is federally listed as threatened, but only actions that occur along coastal areas or large wetland complexes during the migratory window of May 1—September 30, need to be considered to protect this bird, according to USFWS. No suitable habitat for this species is present at or near the Project.

Hine's emerald dragonfly (*Somatochlora hineana*)

Hine's emerald dragonflies are federally listed as endangered and utilize a variety of habitats—most of which are wetland systems. The dragonfly breeds in marshes and sedge meadows that are underlain by dolomite bedrock (magnesia-rich sedimentary rock resembling limestone) and fed by calcareous (calcium carbonate, calcium, or limestone) groundwater seeps. The species requires groundwater-fed rivulets or sheet flow in fens or other wetlands. This Project corridor does not contain any suitable habitat for the Hines Emerald Dragonfly. Critical habitat has been designated by USFWS for this species, but within Illinois is it entirely along the lower Des Plaines River and is not located within or near the project area.

Eastern Massasauga (*Sistrurus catenatus*)

Eastern massasaugas are federally listed as threatened and can be found in a variety of wetland habitats, including bogs, fens, shrub swamps, wet meadows, marshes, moist grasslands, wet prairies, and floodplain forests. They will shift the habitats they use, depending on the season. Generally, they use wetlands in the spring, fall, and winter. In summer, the snakes migrate to drier, upland sites, ranging from forest openings to old fields, agricultural lands, and prairies. Massasaugas usually hibernate in wetlands within crayfish or small mammal burrows. Hibernation sites are located below the frost line, often close to the groundwater level. The presence of water that does not freeze is critical for a suitable hibernaculum. While they are historically known from Cook County, there are no current records in or near the Project, nor is there any suitable habitat present.

Eastern Prairie Fringed Orchid (*Platanthera leucophaea*)

The eastern prairie fringed orchid is federally listed as threatened and occurs in a wide variety of habitats, from mesic prairie to wetlands such as sedge meadows, marsh edges, even bogs. It requires full sun for optimum growth and flowering, and a grassy habitat with little or no woody encroachment. No suitable habitat for this orchid is present within or near the project area.

Leafy prairie clover (*Dalea foliosa*)

Leafy-prairie clover is a federally listed endangered plant species that inhabits prairie remnants on thin soil over dolomitic limestone. No suitable habitat is present within or near the project area.

Prairie bush clover (*Lespedeza leptostachya*)

Prairie bush clover is a federally listed threatened plant species found in dry to mesic prairies with gravelly soil. No suitable habitat is found within the project area.

There is no suitable habitat for any of these federally listed species in or near the Project corridor. No critical habitat is designated within the project area. A determination of “no effect” can be recommended for federally listed species.

6.2 Illinois-Listed Threatened and Endangered Species

The IDNR online Ecological Compliance Assessment Tool (EcoCAT) was also used to determine what potential state-listed species may occur in the project area. EcoCAT is also a geographic-based online system. It provides information on any natural resources of concern to the IDNR, including state-listed species within a project area.

The EcoCAT report dated July 26, 2021, indicated that the Illinois Natural Heritage Database contains no records of state-listed threatened or endangered species in the vicinity of the project area. The EcoCAT report is included in Appendix A. Based on the EcoCAT report, which was submitted for informational purposes only, no further coordination with the IDNR was warranted at this time.

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Section 7

State-Protected Lands

The EcoCAT search also identifies any state-protected lands that may be near the Project including INAI sites, dedicated Nature Preserves, and Land and Water Reserves. No state-protected resources were found in or near the project area according to the EcoCAT report included in Appendix A.

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Section 8

Migratory Birds

Migratory birds are considered a federal trust resource and are protected under the Migratory Bird Treaty Act of 1918. The U.S. Fish and Wildlife Service is the agency charged with conservation and protection of migratory birds at the federal level. Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures.

The IPAC system used to determine what federally listed endangered and threatened species may be in a project area, also provides a list of migratory bird species that are of particular concern to the USFWS because they occur on the Birds of Conservation Concern (BCC) list or warrant special attention in the project area. This is not a list of every migratory bird species that may be present at the Project location under existing conditions.

The migratory bird species and their reason for being included in the list provided by IPAC are given in **Table 8-1**. Where indicated, this list identifies migratory bird species that do not breed within the vicinity of the project area and may only be present during brief migratory windows. In **Table 8-1**, fragmented habitat refers to small clusters of existing habitat that have been divided by other land uses. These areas are too small to meet the requirements of many species of migratory birds. It is referring to the concept known as ecological fragmentation.

Table 8-1: Migratory Bird Species Potentially in Project Area from the IPAC* Database with potential habitat information.

Species	Common Name	Reason for Inclusion in IPAC List	Breeding Habitat	Habitat Presence in Project Area
<i>Ammodramus henslowii</i>	Henslow's Sparrow	BCC** listed.	Grassland	None
<i>Antrostomus vociferus</i>	Eastern Whip-poor-will	BCC listed.	Woodland/Forest	Fragmented, very few mature trees
<i>Aquila chrysaetos</i>	Golden Eagle	Protected under the Eagle Act.	Grassland in Mountains or cliffs	None
<i>Arenaria interpres morinella</i>	Ruddy Turnstone	BCC listed in certain regions.	Breeds elsewhere - coastal habitat	None
<i>Botaurus lentiginosus</i>	American Bittern	Listed in BCC for this region.	Marsh	None
<i>Calcarius pictus</i>	Smith's Longspur	BCC listed in certain regions.	Rare and breeds elsewhere - grasslands for wintering/migration	None
<i>Calidris alpina arctica</i>	Dunlin	BCC listed.	Breeds elsewhere - ponds, rivers, lakes, mudflats.	None

Species	Common Name	Reason for Inclusion in IPAC List	Breeding Habitat	Habitat Presence in Project Area
<i>Calidris pusilla</i>	Semipalmated Sandpiper	BCC listed.	Breeds elsewhere - mudflats, sandbars, ephemeral pools.	None
<i>Calidris subruficollis</i>	Buff-breasted Sandpiper	BCC listed.	Extremely rare and breeds elsewhere - wetlands, rice fields.	None
<i>Coccyzus erythrophthalmus</i>	Black-billed Cuckoo	BCC listed.	Woodlands, thickets	Fragmented
<i>Dendroica cerulea</i>	Cerulean Warbler	BCC listed.	Forest	Fragmented, very few mature trees in a narrow corridor
<i>Dolichonyx oryzivorus</i>	Bobolink	BCC listed.	Grassland	None
<i>Euphagus carolinus</i>	Rusty Blackbird	BCC listed.	Breeds elsewhere - swamps, wet forests	None
<i>Haliaeetus leucocephalus</i>	Bald Eagle	Protected under the Bald and Golden Eagle Protection Act.	Forests near large bodies of water or rivers.	None
<i>Hylocichla mustelina</i>	Wood Thrush	BCC listed.	Forest	Fragmented, very few mature trees in a narrow corridor
<i>Ixobrychus exilis</i>	Least Bittern	BCC listed in certain regions.	Marsh	None
<i>Limnodromus griseus</i>	Short-billed Dowitcher	BCC listed.	Breeds elsewhere - ponds, shores, rivers, lakes.	None
<i>Melanerpes erythrocephalus</i>	Red-headed Woodpecker	BCC listed.	Savanna	None
<i>Oporornis formosus</i>	Kentucky Warbler	BCC listed.	Breeds elsewhere - forest.	None
<i>Pluvialis dominica</i>	American Golden-plover	BCC listed.	Breeds elsewhere - prairie, open fields.	None
<i>Protonotaria citrea</i>	Prothonotary Warbler	BCC listed.	Wet forests, swamps.	None
<i>Rallus elegans</i>	King Rail	BCC listed.	Marsh	None
<i>Tringa flavipes</i>	Lesser Yellowlegs	BCC listed.	Breeds elsewhere - marsh, mudflat, shorelines.	None

*IPAC - The USFWS online Information for Planning and Consultation geographic database accessed at <https://ecos.fws.gov/ipac/location/index> on July 26, 2021.

**BCC - Birds of Conservation Concern list maintained by USFWS.

While this list was generated using the USFWS online IPAC geographically based system, most of these species do not occur in the project area as no suitable habitat is present. The existing habitat present is heavily urbanized, fragmented, and dominated by weedy, non-native, and invasive plant species. Given these conditions, the vast majority of the species listed in **Table 8-1** are unlikely to occur in the project area. Many other migratory bird species are present that are not necessarily species of ecological or conservation concern because they are urban-adapted species. However, these species are still protected under the Migratory Bird Treaty Act. The existing habitat is not a limiting factor for populations of urban adapted species of migratory birds. Observations did not reveal any migratory birds of concern, nor any significant habitat. Many species may fly over this corridor set in a developed urban context, but this project is not expected to have any impact to migratory bird species populations.

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Section 9

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Appendix A
U.S. Fish and Wildlife Service Information for Planning
and Consultation (IPAC) and Illinois Department of Natural
Resources Ecological Compliance Assessment Tool
(EcoCAT) Reports

Applicant: Hey and Associates, Inc.
Contact: Jeffrey Mengler
Address: 26575 W. Commerce Drive, Suite 601
Volo, IL 60073

IDNR Project Number: 2201137
Date: 07/26/2021
Alternate Number: 20-0319

Project: METRA UP North Line bridges
Address: UP North Line Railroad from Fullerton to Addison, Chicago

Description: rehab or replace 12 aging railroad bridges in northside Chicago neighborhoods. Bridge are over streets, NOT waterways.

Natural Resource Review Results

This project was submitted for information only. It is not a consultation under Part 1075.

The Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the project location.

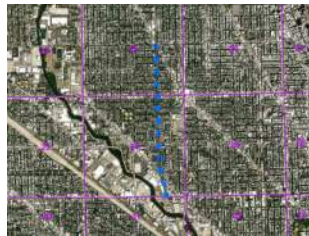
Location

The applicant is responsible for the accuracy of the location submitted for the project.

County: Cook

Township, Range, Section:

40N, 14E, 19
40N, 14E, 30
40N, 14E, 31



IL Department of Natural Resources
Contact
Impact Assessment Section
217-785-5500
Division of Ecosystems & Environment

Government Jurisdiction
Other

Disclaimer

The Illinois Natural Heritage Database cannot provide a conclusive statement on the presence, absence, or condition of natural resources in Illinois. This review reflects the information existing in the Database at the time of this inquiry, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, compliance with applicable statutes and regulations is required.

Terms of Use

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1. The IDNR EcoCAT website was developed so that units of local government, state agencies and the public could request information or begin natural resource consultations on-line for the Illinois Endangered Species Protection Act, Illinois Natural Areas Preservation Act, and Illinois Interagency Wetland Policy Act. EcoCAT uses databases, Geographic Information System mapping, and a set of programmed decision rules to determine if proposed actions are in the vicinity of protected natural resources. By indicating your agreement to the Terms of Use for this application, you warrant that you will not use this web site for any other purpose.

2. Unauthorized attempts to upload, download, or change information on this website are strictly prohibited and may be punishable under the Computer Fraud and Abuse Act of 1986 and/or the National Information Infrastructure Protection Act.

3. IDNR reserves the right to enhance, modify, alter, or suspend the website at any time without notice, or to terminate or restrict access.

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Unauthorized use, tampering with or modification of this system, including supporting hardware or software, may subject the violator to criminal and civil penalties. In the event of unauthorized intrusion, all relevant information regarding possible violation of law may be provided to law enforcement officials.

Privacy

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United States Department of the Interior



FISH AND WILDLIFE SERVICE

Chicago Ecological Service Field Office

U.s. Fish And Wildlife Service Chicago Ecological Services Office

230 South Dearborn St., Suite 2938

Chicago, IL 60604-1507

Phone: (312) 485-9337 Fax:

<http://www.fws.gov/midwest/endangered/section7/s7process/7a2process.html>

In Reply Refer To:

July 26, 2021

Consultation Code: 03E13000-2021-SLI-0698

Event Code: 03E13000-2021-E-01688

Project Name: METRA UP North Line Bridges

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Please note! For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

For all other projects, continue the Section 7 Consultation process by going to our Section 7 Technical Assistance website at <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. If you are familiar with this website, you may want to go to Step 2 of the Section 7 Consultation process at <http://www.fws.gov/midwest/endangered/section7/s7process/step2.html>.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Chicago Ecological Service Field Office

U.s. Fish And Wildlife Service Chicago Ecological Services Office

230 South Dearborn St., Suite 2938

Chicago, IL 60604-1507

(312) 485-9337

Project Summary

Consultation Code: 03E13000-2021-SLI-0698

Event Code: 03E13000-2021-E-01688

Project Name: METRA UP North Line Bridges

Project Type: TRANSPORTATION

Project Description: Rehab or replacement of 12 aging railroad bridges in Chicago northside neighborhoods.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@41.936309949999995,-87.67335603088178,14z>



Counties: Cook County, Illinois

Endangered Species Act Species

There is a total of 8 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Birds

NAME	STATUS
Piping Plover <i>Charadrius melodus</i> Population: [Great Lakes watershed DPS] - Great Lakes, watershed in States of IL, IN, MI, MN, NY, OH, PA, and WI and Canada (Ont.) There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/6039	Endangered
Red Knot <i>Calidris canutus rufa</i> There is proposed critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/1864	Threatened

Reptiles

NAME	STATUS
Eastern Massasauga (=rattlesnake) <i>Sistrurus catenatus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/2202	Threatened

Insects

NAME	STATUS
<p>Hine's Emerald Dragonfly <i>Somatochlora hineana</i></p> <p>There is final critical habitat for this species. The location of the critical habitat is not available.</p> <p>Species profile: https://ecos.fws.gov/ecp/species/7877</p>	Endangered

Flowering Plants

NAME	STATUS
<p>Eastern Prairie Fringed Orchid <i>Platanthera leucophaea</i></p> <p>No critical habitat has been designated for this species.</p> <p>This species only needs to be considered under the following conditions:</p> <ul style="list-style-type: none"> Follow the guidance provided at https://www.fws.gov/midwest/endangered/section7/s7process/plants/epfos7guide.html <p>Species profile: https://ecos.fws.gov/ecp/species/601</p>	Threatened
<p>Leafy Prairie-clover <i>Dalea foliosa</i></p> <p>No critical habitat has been designated for this species.</p> <p>Species profile: https://ecos.fws.gov/ecp/species/5498</p>	Endangered
<p>Prairie Bush-clover <i>Lespedeza leptostachya</i></p> <p>No critical habitat has been designated for this species.</p> <p>Species profile: https://ecos.fws.gov/ecp/species/4458</p>	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

Appendix B

Photographic Log



Photograph 1:

View north at
Wrightwood Avenue
bridge.



Photograph 2:

View south toward
bridge over North
Branch Chicago
River, south of
Fullerton Avenue.



Photograph 3:

View showing proximity of building between Wrightwood Avenue and Diversey Parkway with narrow band of shrubby vegetation.



Photograph 4:

Norway spruce trees planted along west side of railroad near Diversey Parkway.



Photograph 5:
View north showing vegetation along Chi Che Wang Park on east side of railroad.



Photograph 6:
Planting of little bluestem grass (native prairie species) along fence at Chi Che Wang Park.

Page Break



Photograph 7:

Single family homes along west side of railroad with minimal vegetation between Diversey Parkway and Wellington Avenue.



Photograph 8:

View north toward Melrose Street bridge showing typical woody vegetation along both sides of corridor.

Page Break



Photograph 9:

View of garden at base of retaining wall on west side at School Street.



Photograph 10:

View of vegetation along retaining all just north of Addison Street.

Appendix F

Federal Emergency Management Agency Flood Insurance Rate Map

UP North Rebuild: Fullerton to Addison FEMA Flood Insurance Rate Map

Legend

- FEMA National Flood Hazard**
- 1.0% Chance Flood Hazard
 - Area of Minimal Flooding
- Metra**
- UP-N Rail Line
 - Other Metra Rail Lines
 - Metra Rail Stations
- Project Bridge Improvements**
- To Be Repaired/Painted
 - To Be Replaced
 - Project Limits

Key Data Sources:
Federal Emergency Management Administration.
National Flood Hazard Layer.
<https://www.fema.gov/flood-maps/national-flood-hazard-layer>

Location



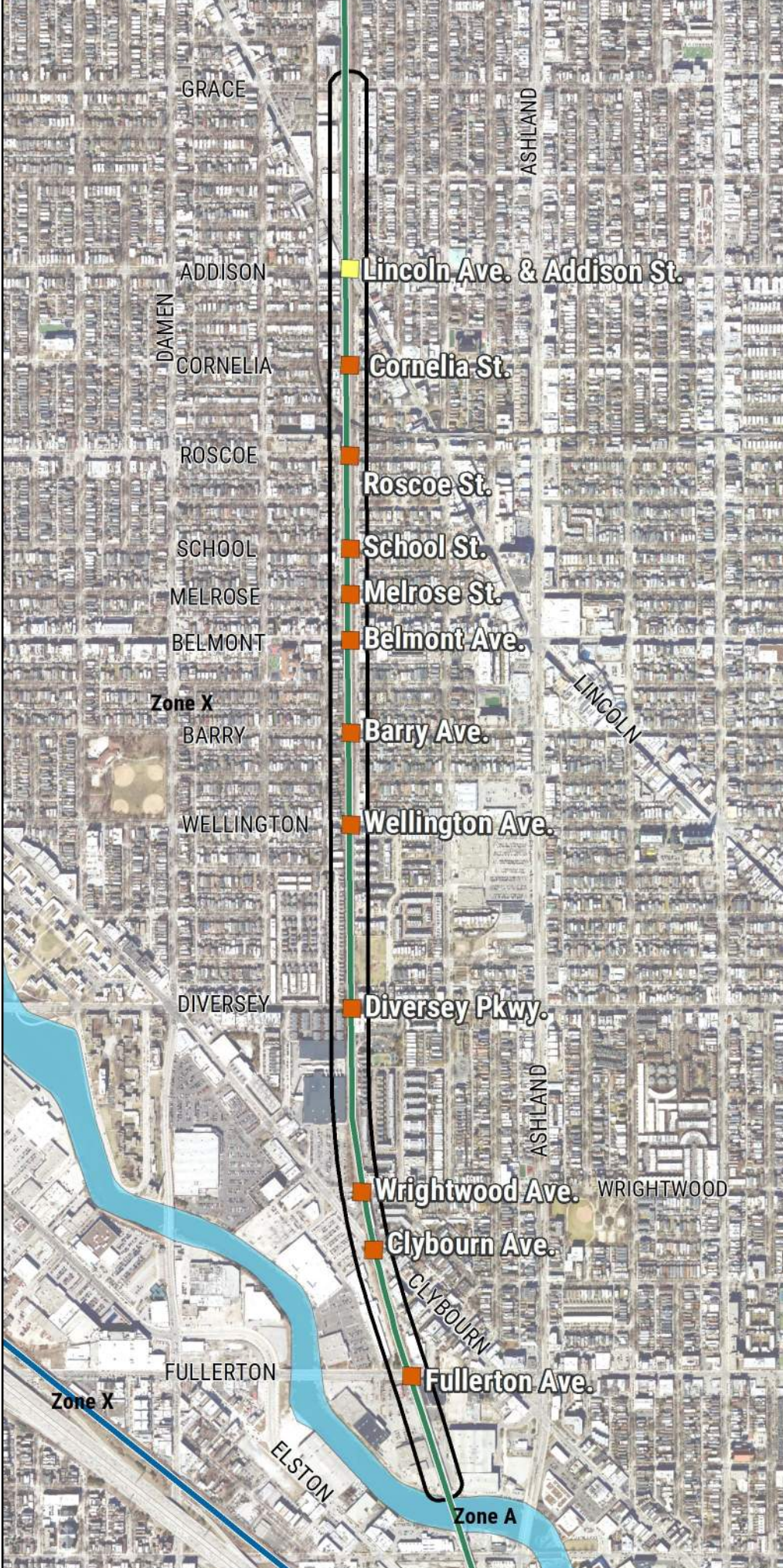
Scale

UP NORTH REBUILD
FULLERTON TO ADDISON

Metra
CDM Smith

N

0 1,000 2,000
Feet



Appendix G

Public Involvement Supporting Documentation

Appendix G-1: Public Participation Plan

Metra Union Pacific (UP) North Rebuild: Fullerton to Addison



Public Participation Plan

Metra Project No. 4840
CDMS Project No. 261845

June 28, 2021

Prepared for:



547 W. Jackson Blvd.
Chicago, IL 60661

Prepared by:



125 S. Wacker Dr.
Chicago, IL 60606

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Appendix A

Section 1

Overview

This section provides an overview of the Project as well as the Public Participation Plan (PPP).

1.1 Project Overview

Metra is proposing to shift the track alignment and replace retaining walls and eleven (11) bridges over roadways along the Union Pacific (UP) North Line from north of the Chicago River Bridge (MP 3.27) to approximately Grace Street (MP 5.22) in Chicago, Illinois. The Project is located just south of the UP North Line Bridge North 11 Project (North 11 Project), which replaced eleven (11) railroad bridges and rebuilt the existing Ravenswood train station. A Documented Categorical Exclusion (DCE) was completed for the North 11 Project in June 2011; construction of the bridges is complete and station construction is ongoing. No property acquisition or changes to the existing railroad right-of-way boundary are anticipated for this Project. A project location map is provided in Figure 1. The Project will include the following elements:

- Replacement of eleven (11) railroad bridges over roadway from Fullerton Avenue to Cornelia Avenue along Metra's UP North Line.
- The roadway will be lowered by approximately one to two feet at Roscoe Street and Cornelia Avenue to maintain adequate vertical clearance for traffic underneath the bridges.
- Replacement of embankment walls and the addition of new retaining walls from Fullerton Avenue to north of Lincoln Avenue/Addison Street along Metra's UP North Line. New retaining walls will be used to keep the Project footprint within the existing right-of-way.
- Lead abatement, prepping and painting for a 12th existing steel railroad bridge at Lincoln Avenue/Addison Street on the UP North Line.
- Adjusting the track alignment to the west to align with the existing tracks at Grace Street Bridge north of the Project limits and the Chicago River Bridge south of the Project limits (all within the existing right-of-way). Raising the track height by up to 3' to maintain adequate vertical clearance, while improving track profile.

In addition, some utility relocation and replacement will be required to accommodate these infrastructure improvements. Existing utilities that are attached to the bridge structures will be replaced. Utilities will be relocated under the Roscoe Street and Cornelia Avenue bridges where streets will be lowered. Where abutments will be replaced at Fullerton Avenue and Clybourn Avenue, some utility work will also occur. Some utility relocations may also occur where new retaining walls are built.

1.2 Project Location

This Project is located entirely within Chicago, Illinois and starts just north of the North Branch Chicago River. The Project begins immediately north of the Deering (Chicago River) Bridge (MP 3.27) and ends at the Grace Street Bridge (MP 5.22). The limits and termini of the Project are defined by the existing infrastructure. Figure 1-1 provides a project location map.

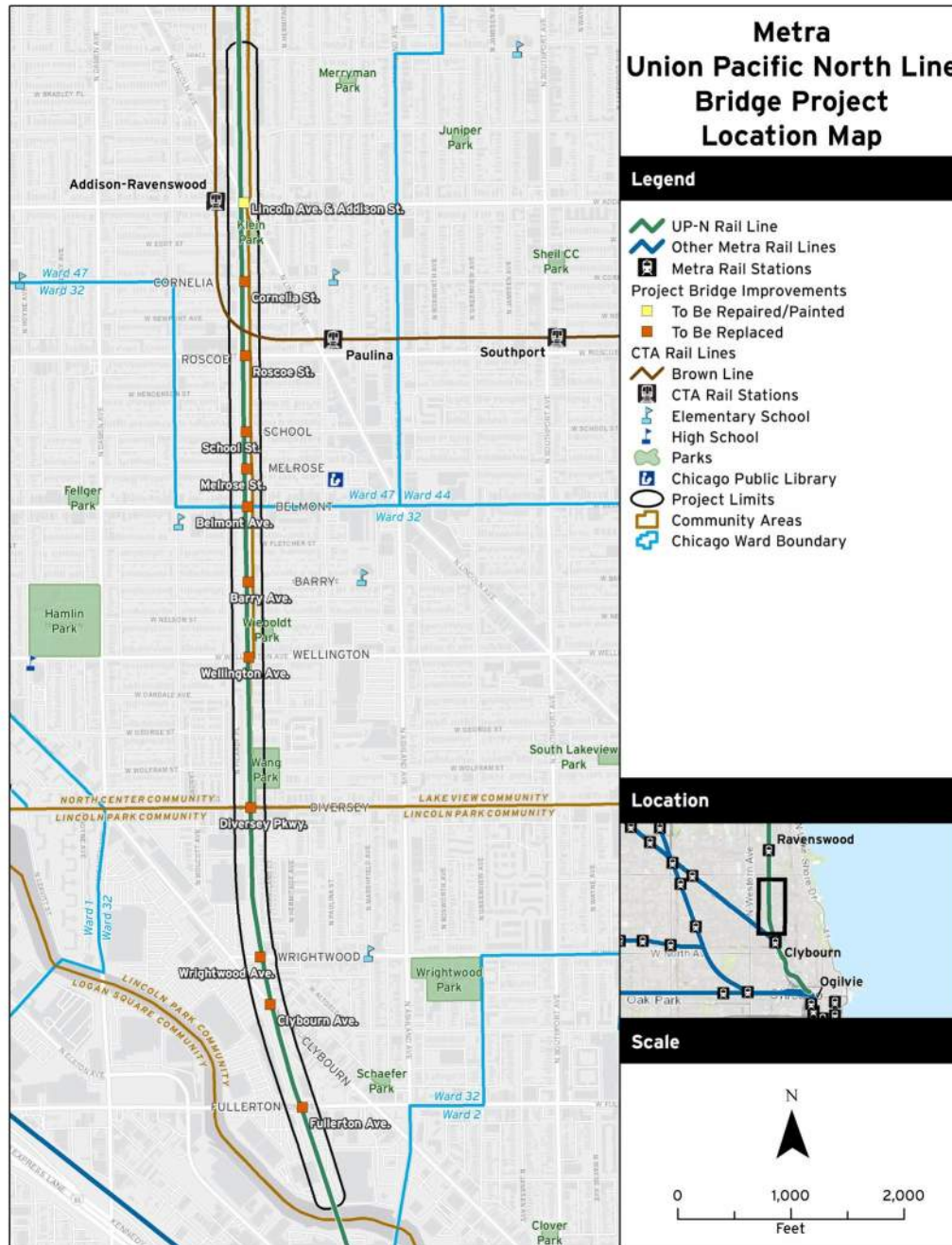


Figure 1-1. Project Location Map

1.3 Documented Categorical Exclusion

Per Class of Action determination by the Federal Transit Administration, Region V, a DCE is being produced for this Project to comply with the National Environmental Policy Act (NEPA). While the bridge replacements are not anticipated to induce significant impacts on the human, natural, or historical environments either individually or cumulatively, there may be community disruptions due to the project and related construction activities. Therefore, considering the potential community disruption and concerns with this project a public outreach process will be followed.

1.4 Public Participation Plan Overview

This Public Participation Plan (PPP) outlines the key public outreach activities expected to be executed as part of this Project. These key activities include aldermanic outreach, stakeholder meetings, property owner coordination, public meetings, and project communications. The plan also outlines the overall outreach schedule of activities. Finally, this document includes an initial stakeholder list. The list is in draft form. Additions will be made throughout the Project as additional groups are identified or organizations come forward to participate in the public input process.

The PPP is a working document that may be updated as the Project progresses. For example, the stakeholder list and outreach schedule may be updated to respond to additional information gathered through outreach efforts. A summary of all outreach activities will be included in the final DCE.

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Section 2

Key Stakeholders

This section provides an overview of key stakeholder activities with Aldermen, community groups, and adjacent property owners.

2.1 Aldermen

Coordination with City of Chicago Aldermen in the project area is an important aspect of successful outreach. It is important to keep the Aldermen and their staff updated on project updates, schedule, and outreach activities. Their offices often receive requests for information or questions from residents regarding infrastructure projects. Providing information to the Aldermen will assist with overall public communication and outreach efforts deployed by Metra.

There are two Aldermanic wards in the project area:

- Ward 32 – Scott Waguespack
- Ward 47 – Matt Martin

Metra will meet with the Aldermen several times throughout the project to provide updates and solicit input. These meetings are intended to be held with the Aldermen and/or staff representative.

Meeting	Anticipated Topic Areas	Timing
Meeting 1	Project introduction/overview Understand potential community concerns and issues Stakeholder list review/suggestions Public Meeting 1 Preview	Summer 2021
Meeting 2	Public / stakeholder / affected property owner feedback to date Public Meeting 2 Preview	Late 2021
Meeting 3	Public Meeting 3 Preview Construction impacts/schedule	TBD, as needed before construction

2.2 Community Groups

Metra will hold several virtual small-group stakeholder meetings with targeted groups in the project area. These meetings will be designed to coordinate on issues specific to potential impacts to adjacent properties (backyards) and key community features (signage/identifier). These small-group stakeholder meetings will provide an opportunity for Metra, project team members, and community group representatives to coordinate potential impacts and mitigations measures. It is anticipated that small group stakeholder meetings will be held with the following:

- Lakeview Roscoe Village Chamber of Commerce – The bridge at Roscoe Street includes a welcome sign for the Roscoe Village neighborhood that will need to be removed when the bridge is replaced. Early coordination to discuss the removal and potential mitigation measures will be required.

- Condo Association Group – Some adjacent condo and other residential properties have placed fences and other items on the existing UP-owned right-of-way. These items will need to be removed as part of this proposed Project. Close coordination with the association(s) and individual property owners (see next section) will be required. Condo association(s) will be identified through site visits, Aldermanic coordination, and additional research.
- Linear Garden Club Group – Small linear garden clubs near and sometimes within the right-of-way that include green space, landscaping and benches. Close coordination with these groups will be required. Garden clubs will be identified through site visits, Aldermanic coordination, and additional research.
- Others – Additional meetings may be needed with other groups as identified throughout the project and in coordination with Aldermanic offices.

2.3 Adjacent Property Owners

This Project includes a unique circumstance wherein the delineation of property lines is not always visible and some adjacent properties may be encroaching into UP right-of-way, utilizing it as part of their backyard or garden area. This Project will require the reconstruction of abutments and construction of new retaining walls. . Therefore, it will be important to closely work with property owners in targeted areas along the corridor to describe the project benefits and potential changes to the adjacent railroad right-of-way.

Metra will conduct targeted outreach to adjacent property owners who may have concerns related to potential impacts to their backyards. Many of these properties are located on the west side of the railroad from approximately Diversey to Belmont.

The Metra Project team will include a dedicated Bilingual Property Owner Liaison (Liaison) who will coordinate and speak directly with these adjacent property owners. This targeted outreach will be conducted primarily through community group or condo association/board contacts and meetings. The Liaison will maintain a log of communications with property owners and brief the Project team on key issues, potential mitigation measures, and next steps. In addition, the Liaison will help identify special situations that may pertain to those impacted by the Project and provide inventory and documentation of the characteristics and needs of the residences.

The project team will direct inquiries received via email and public meetings related to these properties directly to the Liaison. The Liaison will also be available to coordinate directly with homeowners' associations/representatives during public meetings and throughout the project.

2.4 Stakeholder List

A stakeholder list will be maintained throughout the Project. This list will include stakeholder contact information for the key stakeholders listed in the previous sections. It will also include other community groups, residents, and others who wish to receive project notifications (e.g. public meetings). The list will be updated throughout the Project. A draft list is included in the Appendix.

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Section 3

Public Meetings

This section describes the activities anticipated related to public meetings. Three public meetings are expected for this Project.

3.1 Topics & Timing

Metra will host three public meetings for this project. The meetings will provide an opportunity for Metra to share project information and updates with the public, as well as collect feedback. The anticipated topic areas and content for each public meeting are described below. Topic areas will be refined as the Project develops based on additional design, engineering, and environmental analysis. Similarly, topic areas will be refined or adjusted based on feedback received through aldermanic and/or stakeholder meetings held before public meetings to ensure topics covered provide adequate information for the public and address key community concerns and issues.

Meeting	Anticipated Topic Areas	Timing
Meeting 1 – Project Introduction	Project introduction/overview Purpose/need Initial plans Solicit community concerns and issues to identify targeted outreach Project timeline	Late Summer/Early Fall 2021
Meeting 2 – Responses & Recommendations	Present the options in response to previous feedback Potential environmental impacts Initial/potential construction impacts Response measures/recommendations Project timeline	Late 2021
Meeting 3 – Pre-Construction Briefing	Abbreviated public meeting Potentially held in coordination with an existing Aldermanic community meeting or stakeholder meeting Communicate construction activities and schedule	TBD, as needed before construction

3.2 Meeting Format & Materials

Metra anticipates hosting one virtual meeting for each round of discussions (three virtual meetings total). The virtual meetings will include a presentation with project information, maps, infographics and other renderings or visualizations as needed to help describe project elements. Spanish translations of materials will be available for the public meetings.

While the meetings will be planned as virtual, there is a possibility to include a small in-person “office hours” or similar for individuals to meet directly with project staff and the Liaison to discuss the project and address potential questions. This effort would be in tandem with the virtual meetings and will be determined if needed closer to the public meeting date due to the changing nature of local/state in-person meeting restrictions and guidelines.

Comments, questions, and feedback will be accepted during the public meeting and for a period of up to two weeks following the meeting date to be included in the meeting notes. The presentations and meeting material will be posted to the Project website following each meeting. Answers to frequently asked questions (FAQ) received during the meetings will also be posted on the Project website (see Project Communications section).

3.3 Notifications

Metra will use several mediums to provide public meeting notifications. Materials will be provided in English and Spanish. These include:

- Press Release – A press release will be developed for each public meeting and sent to Metra’s media list before meeting dates.
- Newspaper Advertisement – A newspaper ad will be placed to promote the meetings.
- Social Media Content and Partner Kits – A meeting flyer and project information will be provided for partners (aldermen, stakeholders, etc.) to promote virtual public meetings and project information via their listservs, social media and websites.
- Flyer – A meeting flyer will be posted at Ravenswood and Clybourn Metra stations, as well as the local library and Alderman’s offices. The flyer will also be sent to community groups, as noted above. Meeting flyers may be placed on UP-N trains, pending UP approval.
- Yard Signs – Place yard signs along the corridor to promote the project, including project website and contact information. Direct residents to website for public meeting and detailed project information.

Section 4

Project Communications

The section below describes key project communication efforts and materials, including project branding, fact sheet, website, and responses to public comments and questions.

4.1 Project Name and Branding

The use of a prominent project name and logo helps provide consistency across project materials and communications and project visibility. After development, the Project name and logo will be incorporated on all public-facing project materials including, but not limited to, the project website, fact sheets, and public meeting materials.

4.2 Project Fact Sheet

A project fact sheet will be developed to help communicate consistent project information and messages. The fact sheet will include a general project overview, key elements/features, map, and schedule. It will also include the project website and email address for additional information and to submit questions/comments. The project fact sheet is intended for use at aldermanic, stakeholder, and public meetings. The fact sheet will also be posted on the project website and utilized in the “Partner Kits” described in Section 3. The fact sheet will be translated into Spanish.

4.3 Project Website

A project website will be developed to provide stakeholders, residents, and the general public with updated project information and materials. The website will include general project overview, map, schedule, including anticipated public meeting timing. It will also include the project fact sheet. The website will be updated before and after each public meeting to post public meeting notices and meeting materials. In addition, the website can be maintained after this portion of the project is complete to update construction timing and potential impacts as well. The website address will be incorporated in all project materials.

4.4 Public Comments and Questions

A project-specific email address will be developed. The email address will be posted on the project website and included in public-facing materials. The email will provide an opportunity for the public to ask questions or submit comments. The project team will review and record emails, responding as appropriate to queries and clarifications either directly or through a FAQ document posted to the project website and disseminated to commenters/meeting attendees. The FAQ will be provided in Spanish and English.

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Section 5

Environmental Justice Considerations

An environmental justice analysis is being conducted in accordance with related federal and state laws and guidance including Title VI of the 1964 Civil Rights Act, Executive Order 12898, Executive Order 13166, and FTA Circulars 4703.1 and 4702.1B will be conducted as part of the DCE for this Project.

While no disproportionate impacts to environmental justice communities are expected as part of this project, US Census data was reviewed for environmental justice and limited English-speaking populations for public outreach considerations.

5.1 Review Conducted

A detailed environmental justice analysis and review is being conducted. The results will be reviewed to determine what, if any, Environmental Justice communities exist in the project area. If environmental justice communities are present in the project area, additional outreach approaches or activities would be considered. At a minimum, this could include targeted meeting notifications or small group stakeholder meetings with environmental justice communities. This section will be updated after the review is complete.

A preliminary review of limited English-speaking populations was conducted using the U.S. Environmental Protection Agency's EJScreen tool (www.epa.gov/ejscreen). A review of data within ½ mile of the project area was conducted. The results are included in the Appendix.

Of the population age five and older, 88% speak only English at home. Spanish is the most common language besides English spoken at home, at 5%. The remaining include small percentages of other languages. Of those who speak non-English at home, 9% speak English "very well". There are an estimated 376 linguistically isolated households, which makes up less than 2% of the estimated 20,585 households in the ½ mile area. Spanish is the most common language for linguistically isolated households.

5.2 Recommendations

Providing materials in Spanish is recommended. This includes public meeting notifications and project fact sheet. In addition, a Spanish interpreter/translator should be provided at public meetings. The affected property owner liaison is also bilingual to provide consistent communication services to Spanish-speaking property owners. The project website will provide an embedded language translation tool at the top of each page.

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Appendix A

Public Participation Calendar

	2021							2022							
Month	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug
Activity															
Public Outreach Plan	X	X													
Round 1															
Aldermanic Meeting			X												
Stakeholder Meetings				X											
Public Meeting 1				X											
Round 2															
Aldermanic Meeting 2						X	X								
Stakeholder Meetings							X								
Affected Property Owner Outreach (if needed)							X								
Public Meeting 2							X								
Round 3															
Aldermanic Meeting 3															
Public Meeting 3															
	TBD – Pre-Construction Mtgs														

Name	Contact
Aldermanic Wards	
Ward 47 – Matt Martin	Matt Martin and Josh Mark
Ward 32 – Scott Waguespack	Paul Sajovec
Ward 2 - Brian Hopkins	Brian Hopkins
Chambers of Commerce	
Northcenter Chamber of Commerce	Eileen Keleghan
Lakeview/Roscoe Village Chamber of Commerce	Becca Girsch
Lincoln Park Chamber	Kim Schlif
Greater Ravenswood Chamber of Commerce	Megan Bunimovich
North Branch Works	Steve Simmons
Neighborhood/Community Groups	
West Lakeview Neighbors	Jeff Goad
Roscoe Village Neighbors	Ryan Jacox and Lawrence Peterson
North Center / Lincoln Square Neighbors	Renée Girardin
South Lakeview Neighbors	Bob Blitstein
Sheffield Neighborhood Association	Brian Comer
West DePaul Neighborhood Association	David Haymes
Montrose Metra Community Gardens / Neighbor Space	Kasey (Bersett) Eaves
Winnslie Parkway	Friends of Winnslie Parkway
Bucktown Community Organization	Eva Bergant (Vice President)
Wicker Park Committee	Kyle Sneed
Ranch Triangle Association	Erma Tranter
Gross Park Neighbors	John Meyer
Unknown Gardener	Keith Krisciunas/Julie Gross
Schools	
Friedrich L Jahn Public School	Michael Herring
Lakeview High School	Maria Ramirez
Hamilton Elementary School	Denise Pfeifer
Augustus H. Burley Elementary School	Catherine Plocher
Oscar F. Mayer School	Danielle Drayton
Alcott College Prep School	Nancy Lugardo
John James Audubon Elementary School	Meghan Duffy
Alexander Graham Bell School	Kathleen Miller
Blaine Elementary School	Andrew Russell
John C. Coonley School	Brennan Humphrey
Lane Tech College Prep School	Edwina Thompson
William H. Prescott Elementary School	Erin Roche
DePaul University	Kathryn Stieber
Library	
Chicago Public Library - Lincoln Belmont Branch	N/A
Chicago Public Library - Lincoln Park Branch	N/A

Name	Contact
Agencies	
Illinois Department of Transportation	Holly Bieneman
Chicago Metropolitan Agency for Planning	Tom Murtha
Chicago Department of Transportation	Joe Alonzo, Lubica Benak, Jeffrey Sriver
Chicago Park District	Heather Gleason
Chicago Transit Authority	Greg Longhini
Federal Transit Administration	Elizabeth Breiseth and Anthony Greep
Regional Transportation Authority	Leanne Redden
Cook County Department of Transportation and Highways	Jennifer "Sis" Killen
Elected Officials	
IL House District 11 - State Representative - Ann Williams	Hannah Kreischer
IL Senate District 6 - State Senator - Sara Feigenholtz	Sara Feigenholtz
US Senator - IL - Richard Durbin	Clarisol Duque
US Senator - IL - Tammy Duckworth	Kalina Thompson
US Congress - IL District 5 - Mike Quigley	Charlie Chamness and Dana Fritz
US Congress - IL District 9 - Jan Schakowsky	Syd Terry
US Congress - IL District 10 - Brad Schneider	Casey O'Shea
Other	
Union Pacific Railroad	Liisa Stark and Lindsay Douglass
Adjacent Homeowners and Condo Associations	
Picardy Place	N/A
Wolcott Diversey Condo Association	N/A
Lofts at 1800 Condo Association	N/A
Hartland Park Townhomes	N/A
Regal Loft Condos	N/A
Terra Cotta Village Condominium	N/A
Landmark Village Condominiums	N/A
3216 N Ravenswood Avenue	N/A
1738 W Diversey Pkwy.	N/A
1743-1751 Terra Cotta Pl.	N/A
1751 W Roscoe St.	N/A
1753 W Cornelia Ave.	N/A
1754 W Wellington Ave.	N/A
1755 W. Roscoe St.	N/A
1756 W Cornelia Ave.	N/A
1758 W Diversey Pkwy.	N/A
1760 W Diversey Pkwy.	N/A
1760 W Wrightwood Ave.	N/A

Name	Contact
3201 N Ravenswood Ave.	N/A
3223 N Ravenswood Ave.	N/A
3233-3245 N Ravenswood Ave.	N/A
The Maynard at 2500 N Clybourn Ave	N/A
Wellington Park Homeowners Association	N/A
1801 W Addison St.	N/A
1808 W Belmont Ave.	N/A
1810 W Belmont Ave,	N/A
1814 W Belmont Ave.	N/A
1818-1820 W Wellington Ave.	N/A
2929 N Honore St.	N/A
3125 to 3139 N Honore St.	N/A
Townhomes at 3141-3159 Honore St. and 1801-1809 Belmont Ave.	N/A
3212 N Ravenswood Ave.	N/A
3312 N Ravenswood Ave.	N/A
3320 N Ravenswood Ave.	N/A
3358 N Ravenswood Ave.	N/A

EPA EJScreen



EJSCREEN ACS Summary Report



Location: User-specified linear location
 Ring (buffer): .5-miles radius
 Description: Metra UP N Bridges

Summary of ACS Estimates		2014 - 2018	
Population		40,048	
Population Density (per sq. mile)		18,705	
People of Color Population		10,180	
% People of Color Population		20%	
Households		20,585	
Housing Units		23,241	
Housing Units Built Before 1950		11,935	
Per Capita Income		77,812	
Land Area (sq. miles) (Source: SF1)		2.67	
% Land Area		98%	
Water Area (sq. miles) (Source: SF1)		0.05	
% Water Area		2%	

	2014 - 2018 ACS Estimates	Percent	MOE (t)
Population by Race			
Total	40,048	100%	498
Population Reporting One Race	48,147	96%	1,211
White	43,477	87%	470
Black	1,354	3%	339
American Indian	88	0%	49
Asian	2,671	5%	218
Pacific Islander	7	0%	14
Some Other Race	550	1%	121
Population Reporting Two or More Races	1,801	4%	166
Total Hispanic Population	4,397	9%	265
Total Non-Hispanic Population	45,551		
White Alone	39,759	80%	466
Black Alone	1,323	3%	339
American Indian Alone	76	0%	49
Non-Hispanic Asian Alone	2,663	5%	218
Pacific Islander Alone	7	0%	14
Other Race Alone	49	0%	30
Two or More Races Alone	1,674	3%	166
Population by Sex			
Male	25,886	52%	329
Female	24,063	48%	251
Population by Age			
Age 0-4	4,560	9%	193
Age 0-17	10,779	22%	239
Age 18+	39,170	78%	313
Age 65+	2,624	5%	106

Data Note: Detail may not sum to totals due to rounding. Hispanic population can be of any race.
 N/A means not available. Source: U.S. Census Bureau, American Community Survey (ACS) 2014 - 2018.

June 24, 2021

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EJSCREEN ACS Summary Report



Location: User-specified linear location
 Ring (buffer): 5-miles radius
 Description: Metra UP N Bridges

	2014 - 2018 ACS Estimates	Percent	MOE (±)
Population 25+ by Educational Attainment			
Total	34,969	100%	272
Less than 9th Grade	593	2%	73
9th - 12th Grade, No Diploma	407	1%	82
High School Graduate	2,029	6%	104
Some College, No Degree	3,526	10%	125
Associate Degree	749	2%	73
Bachelor's Degree or more	28,414	81%	232
Population Age 5+ Years by Ability to Speak English			
Total	45,388	100%	439
Speak only English	40,002	88%	347
Non-English at Home ^{1,2,3,4}	5,387	12%	176
² Speak English "very well"	4,145	9%	156
² Speak English "well"	767	2%	83
² Speak English "not well"	350	1%	85
⁴ Speak English "not at all"	124	0%	50
^{3,4} Speak English "less than well"	475	1%	85
^{2,3,4} Speak English "less than very well"	1,242	3%	108
Linguistically Isolated Households*			
Total	376	100%	52
Speak Spanish	208	55%	40
Speak Other Indo-European Languages	127	34%	47
Speak Asian-Pacific Island Languages	42	11%	15
Speak Other Languages	0	0%	11
Households by Household Income			
Household Income Base	20,585	100%	145
< \$15,000	887	4%	79
\$15,000 - \$25,000	589	3%	45
\$25,000 - \$50,000	1,747	8%	101
\$50,000 - \$75,000	2,148	10%	100
\$75,000 +	15,214	74%	156
Occupied Housing Units by Tenure			
Total	20,585	100%	145
Owner Occupied	10,939	53%	119
Renter Occupied	9,647	47%	124
Employed Population Age 16+ Years			
Total	39,775	100%	325
In Labor Force	33,626	85%	278
Civilian Unemployed in Labor Force	968	2%	89
Not In Labor Force	6,149	15%	182

Data Note: Detail may not sum to totals due to rounding. Hispanic population can be of any race.

N/A means not available. Source: U.S. Census Bureau, American Community Survey (ACS)

*Households in which no one 14 and over speaks English "very well" or speaks English only.

June 24, 2021

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EJSCREEN ACS Summary Report



Location: User-specified linear location
 Ring (buffer): .5-miles radius
 Description: Metra UP N Bridges

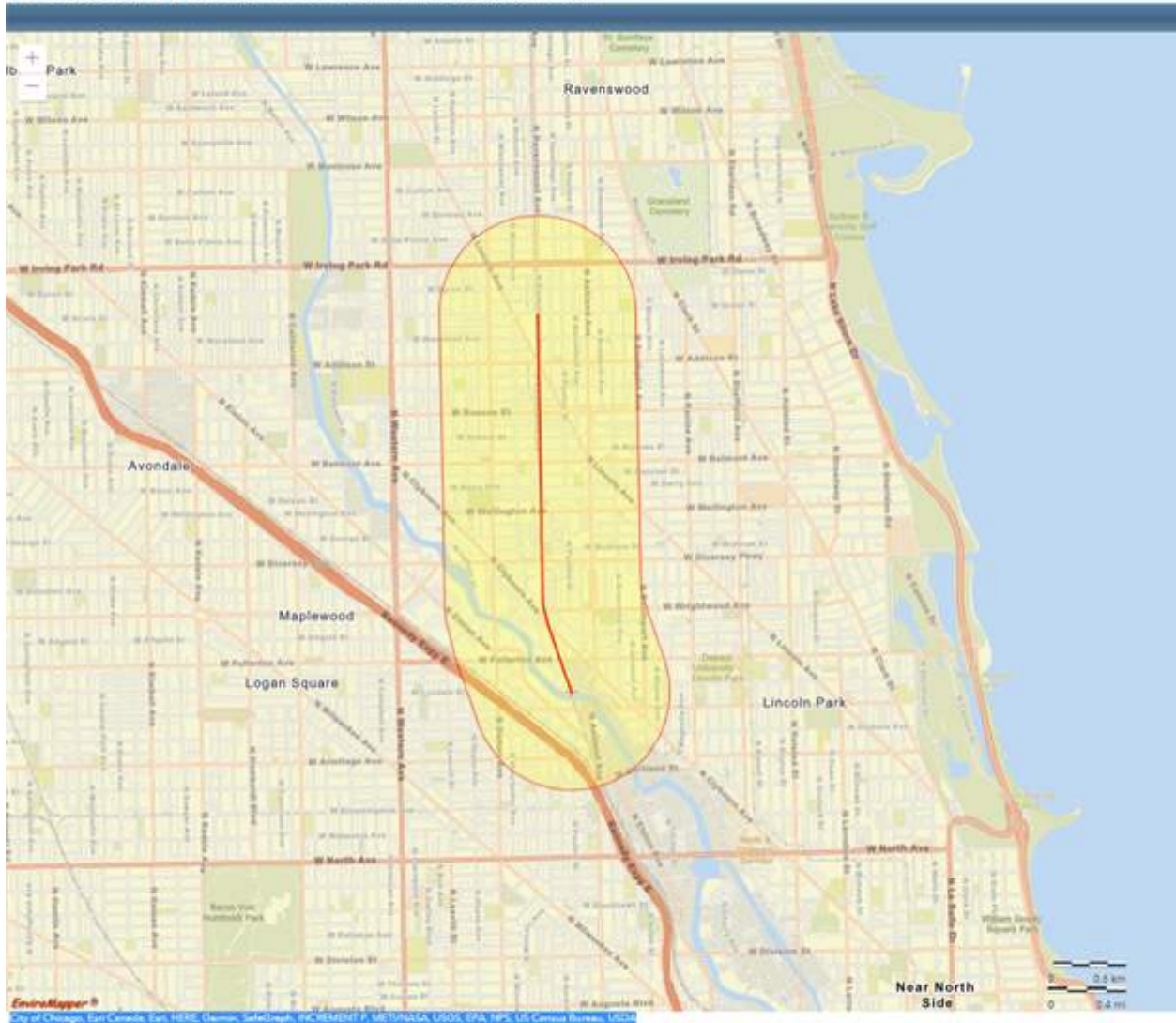
	2014 - 2018 ACS Estimates	Percent	MOE (±)
Population by Language Spoken at Home*			
Total (persons age 5 and above)	47,289	100%	514
English	41,670	88%	484
Spanish	2,523	5%	178
French	354	1%	92
French Creole	N/A	N/A	N/A
Italian	N/A	N/A	N/A
Portuguese	N/A	N/A	N/A
German	204	0%	60
Yiddish	N/A	N/A	N/A
Other West Germanic	N/A	N/A	N/A
Scandinavian	N/A	N/A	N/A
Greek	N/A	N/A	N/A
Russian	N/A	N/A	N/A
Polish	N/A	N/A	N/A
Serbo-Croatian	N/A	N/A	N/A
Other Slavic	N/A	N/A	N/A
Armenian	N/A	N/A	N/A
Persian	N/A	N/A	N/A
Gujarathi	N/A	N/A	N/A
Hindi	N/A	N/A	N/A
Urdu	N/A	N/A	N/A
Other Indic	N/A	N/A	N/A
Other Indo-European	888	2%	92
Chinese	215	0%	42
Japanese	N/A	N/A	N/A
Korean	56	0%	32
Mon-Khmer, Cambodian	N/A	N/A	N/A
Hmong	N/A	N/A	N/A
Thai	N/A	N/A	N/A
Laotian	N/A	N/A	N/A
Vietnamese	32	0%	32
Other Asian	185	0%	45
Tagalog	292	1%	123
Other Pacific Island	N/A	N/A	N/A
Navajo	N/A	N/A	N/A
Other Native American	N/A	N/A	N/A
Hungarian	N/A	N/A	N/A
Arabic	59	0%	27
Hebrew	N/A	N/A	N/A
African	N/A	N/A	N/A
Other and non-specified	191	0%	57
Total Non-English	5,619	12%	706

Data Note: Detail may not sum to totals due to rounding. Hispanic population can be of any race.
 N/A means not available. **Source:** U.S. Census Bureau, American Community Survey (ACS) 2014 - 2018.
 *Population by Language Spoken at Home is available at the census tract summary level and up.

June 24, 2021

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EPA EJSCREEN EPA's Environmental Justice Screening and Mapping Tool (Version 2020)



Appendix G

Public Involvement Supporting Documentation

Appendix G-2: Stakeholder List

Name	Contact
Aldermanic Wards	
Ward 47 – Matt Martin	Matt Martin and Josh Mark
Ward 32 – Scott Waguespack	Paul Sajovec
Ward 2 - Brian Hopkins	Brian Hopkins
Chambers of Commerce	
Northcenter Chamber of Commerce	Eileen Keleghan
Lakeview/Roscoe Village Chamber of Commerce	Becca Girsch
Lincoln Park Chamber	Kim Schlif
Greater Ravenswood Chamber of Commerce	Megan Bunimovich
North Branch Works	Steve Simmons
Neighborhood/Community Groups	
West Lakeview Neighbors	Jeff Goad
Roscoe Village Neighbors	Ryan Jacox and Lawrence Peterson
North Center / Lincoln Square Neighbors	Renée Girardin
South Lakeview Neighbors	Bob Blitstein
Sheffield Neighborhood Association	Brian Comer
West DePaul Neighborhood Association	David Haymes
Montrose Metra Community Gardens / Neighbor Space	Kasey (Bersett) Eaves
Winnslie Parkway	Friends of Winnslie Parkway
Bucktown Community Organization	Eva Bergant (Vice President)
Wicker Park Committee	Kyle Sneed
Ranch Triangle Association	Erma Tranter
Gross Park Neighbors	John Meyer
Unknown Gardener	Keith Krisciunas/Julie Gross
Schools	
Friedrich L Jahn Public School	Michael Herring
Lakeview High School	Maria Ramirez
Hamilton Elementary School	Denise Pfeifer
Augustus H. Burley Elementary School	Catherine Plocher
Oscar F. Mayer School	Danielle Drayton
Alcott College Prep School	Nancy Lugardo
John James Audubon Elementary School	Meghan Duffy
Alexander Graham Bell School	Kathleen Miller
Blaine Elementary School	Andrew Russell
John C. Coonley School	Brennan Humphrey
Lane Tech College Prep School	Edwina Thompson
William H. Prescott Elementary School	Erin Roche
DePaul University	Kathryn Stieber
Library	
Chicago Public Library - Lincoln Belmont Branch	N/A
Chicago Public Library - Lincoln Park Branch	N/A

Name	Contact
Agencies	
Illinois Department of Transportation	Holly Bieneman
Chicago Metropolitan Agency for Planning	Tom Murtha
Chicago Department of Transportation	Joe Alonzo, Lubica Benak, Jeffrey Sriver
Chicago Park District	Heather Gleason
Chicago Transit Authority	Greg Longhini
Federal Transit Administration	Elizabeth Breiseth and Anthony Greep
Regional Transportation Authority	Leanne Redden
Cook County Department of Transportation and Highways	Jennifer "Sis" Killen
Elected Officials	
IL House District 11 - State Representative - Ann Williams	Hannah Kreischer
IL Senate District 6 - State Senator - Sara Feigenholtz	Sara Feigenholtz
US Senator - IL - Richard Durbin	Clarisol Duque
US Senator - IL - Tammy Duckworth	Kalina Thompson
US Congress - IL District 5 - Mike Quigley	Charlie Chamness and Dana Fritz
US Congress - IL District 9 - Jan Schakowsky	Syd Terry
US Congress - IL District 10 - Brad Schneider	Casey O'Shea
Other	
Union Pacific Railroad	Liisa Stark and Lindsay Douglass
Adjacent Homeowners and Condo Associations	
Picardy Place	N/A
Wolcott Diversey Condo Association	N/A
Lofts at 1800 Condo Association	N/A
Hartland Park Townhomes	N/A
Regal Loft Condos	N/A
Terra Cotta Village Condominium	N/A
Landmark Village Condominiums	N/A
3216 N Ravenswood Avenue	N/A
1738 W Diversey Pkwy.	N/A
1743-1751 Terra Cotta Pl.	N/A
1751 W Roscoe St.	N/A
1753 W Cornelia Ave.	N/A
1754 W Wellington Ave.	N/A
1755 W. Roscoe St.	N/A
1756 W Cornelia Ave.	N/A
1758 W Diversey Pkwy.	N/A
1760 W Diversey Pkwy.	N/A
1760 W Wrightwood Ave.	N/A

Name	Contact
3201 N Ravenswood Ave.	N/A
3223 N Ravenswood Ave.	N/A
3233-3245 N Ravenswood Ave.	N/A
The Maynard at 2500 N Clybourn Ave	N/A
Wellington Park Homeowners Association	N/A
1801 W Addison St.	N/A
1808 W Belmont Ave.	N/A
1810 W Belmont Ave,	N/A
1814 W Belmont Ave.	N/A
1818-1820 W Wellington Ave.	N/A
2929 N Honore St.	N/A
3125 to 3139 N Honore St.	N/A
Townhomes at 3141-3159 Honore St. and 1801-1809 Belmont Ave.	N/A
3212 N Ravenswood Ave.	N/A
3312 N Ravenswood Ave.	N/A
3320 N Ravenswood Ave.	N/A
3358 N Ravenswood Ave.	N/A

Appendix G

Public Involvement Supporting Documentation

Appendix G-3: Public Outreach Summary Report

Metra Union Pacific (UP) North Rebuild: Fullerton to Addison



Public Outreach Summary Report

Metra Project No. 4840
CDMS Project No. 261845

June 7, 2022

Prepared for:



547 W. Jackson Blvd.
Chicago, IL 60661

Prepared by:



125 S. Wacker Dr.
Suite 700
Chicago, IL 60606



714 S. Dearborn St.
Floor 3
Chicago, IL 60605

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Section 1: Overview

This section provides an overview of the public outreach process.

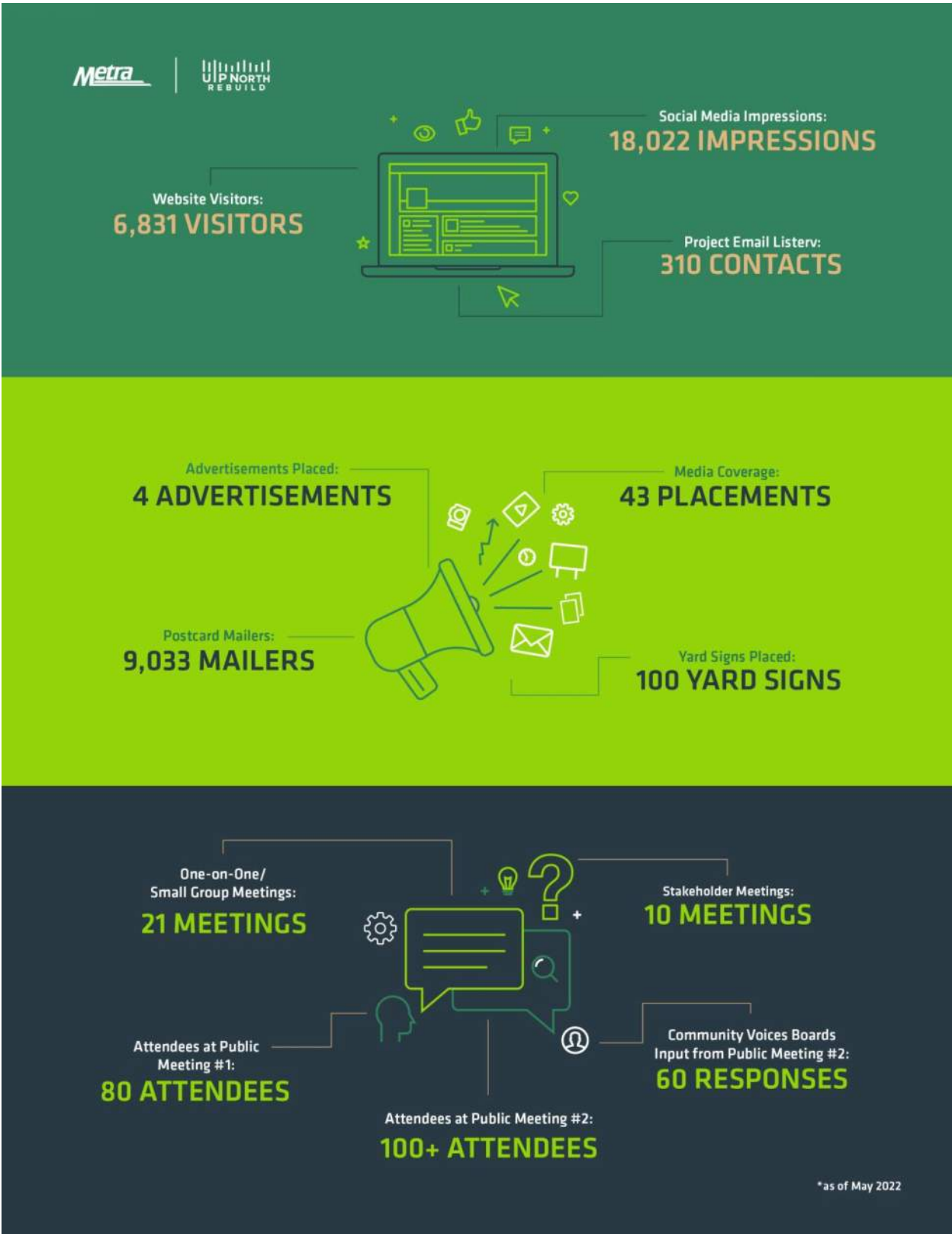
1.1 Outreach Overview

The purpose of this summary report is to document the stakeholder and public outreach conducted as part of the Metra UP North Rebuild Addison to Fullerton Project. The Project outreach activities generally followed the Public Participation Plan which was created at the beginning of the Project.

This report includes a summary of outreach conducted from the start of the Project in mid-2021 up to and briefly following Public Meeting #2 on April 27, 2022. Following Public Meeting #2, the public was encouraged to submit comments and questions through May 23, 2022, so that input could be incorporated into the National Environmental Policy Act (NEPA) environmental review process and be considered during the final design and engineering process. All comments received following this period were recorded and documented.

Additional public outreach will continue as the project progresses. A pre-construction meeting is anticipated in 2023 to inform the community of proposed construction plans and how the Project work could affect them. Additional coordination efforts with stakeholders such as the alderman's offices, local community groups, and adjacent residents will to be conducted on an as-needed basis.

The below graphic summarizes the outreach and engagement conducted for the environmental review process through publication of this report.



Section 2: Project Communications

The section below describes key Project communication efforts and materials, including the Project branding, fact sheet, and website that were developed for the Project.

2.1 Project Name and Branding

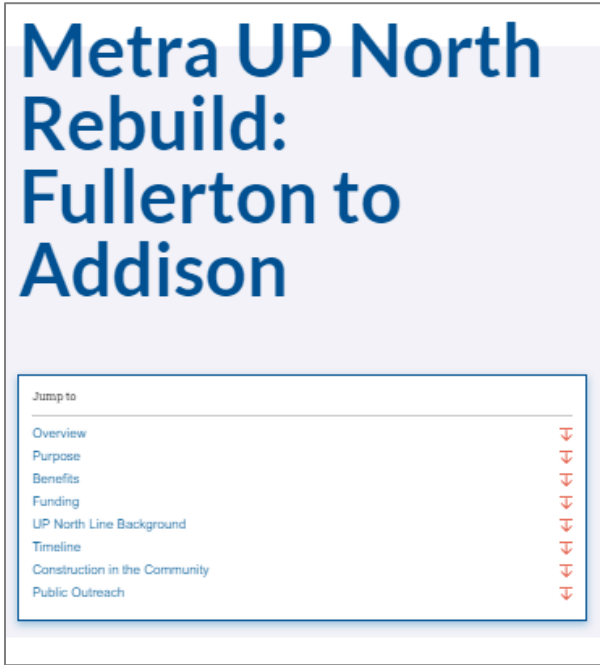
A Project name and logo were developed to provide consistency across Project materials and communications as well as create Project visibility. The Project name and logo is incorporated on all public-facing materials.



2.2 Project Website

A Project website (metra.com/UPNrebuild) was developed to provide stakeholders, residents, and the public with updated Project information and materials. The website includes general Project details such as an overview, map, and anticipated schedule, a frequently asked questions (FAQ) section, Project updates, and all information that was presented and used at each of the two public meetings. The website was first created in August 2021. Major website updates occurred following Public Meeting #1 in September 2021, February 2022, April 2022 and following Public Meeting #2 in May 2022 to provide new materials for the public and encourage public participation. As of Friday, May 27, 2022, there had been 6,831 visitors to the website.

Project Website – September 2021



2.2.1 Frequently Asked Questions (FAQ) Section

A main component of the Project website was the development of an FAQ section. This section served as an opportunity to address many of the most commonly asked questions and concerns received from the public. These questions were gathered from questions asked during the virtual Public Meeting #1, submitted emails, coordination between residents and the adjacent resident liaison, individual or small group meetings with the Project team, and Public Meeting #2 via comment cards, the Question and Answer (Q&A) forum, and individual correspondence with team members.

As of May 2022, over 90 answers to FAQs have been added to the website to address public questions. The FAQ section was initially added shortly following Public Meeting #1. Additional questions and answers were added in February 2022, April 2022, and May 2022.

2.3 Project Email

A Project email address (upnrebuild@metrarr.com) and listserv was developed to provide a direct line of communication with individuals interested in the Project. From mid-2021 up to May 23, 2022, emails were sent to the project listserv to provide the public with relevant updates and to encourage individuals to sign up for outreach efforts such as the public meetings. All questions and comments received via email have been tracked and documented.

As of May 23, 2022, 310 individuals have been added to the Project email listserv. A total of 11 project emails have been sent so far to the listserv with relevant Project updates and requests for participation.

Section 3: Key Stakeholders

This section provides an overview of key stakeholder activities with Aldermen, City of Chicago departments, a U.S. congressional office and community groups.

3.1 Governmental Stakeholders

Coordination meetings were conducted with various governmental stakeholders including the 5th District U.S. Representative Mike Quigley's office, the City of Chicago's Department of Transportation (CDOT) and Department of Planning and Development, and Ward 32 (Ald. Scott Waguespack) and Ward 47 (Ald. Matt Martin) aldermanic offices.

- U.S. Representative 5th District Summary – On April 7, 2022, a coordination meeting was held with Congressman Mike Quigley's office, who was represented by Charlie Chamness and Dana Fritz. The meeting was held to introduce the proposed Project and discuss public outreach efforts. Congressman Quigley's office has received numerous public comments about the Project.
- CDOT Summary – On February 3, 2022, The Project team met with Joe Alonzo, Lubica Benak, and Jeffrey Sriver of CDOT to introduce the proposed Project. The meeting focused on potential opportunities for a new community identifier to replace the Roscoe Village sign, landscaping needs and opportunities within the public right of way, and proposed construction activities at underpasses of the bridge locations.
- Chicago Department of Planning Summary – On March 28, 2022, the Project team met with Josh Mark and Michael Sewall from the Ward 32 alderman's office and Will Holland and Meg Gustafson of the Chicago Department of Planning. The purpose of the meeting was to discuss potential options to minimize trees and vegetation removal along the Project corridor and what opportunities may be available to increase trees and vegetation along the corridor following construction.
- Ward 32 Summary – Coordination meetings were conducted with representatives from Ward 32 alderman's office on August 10, 2021, January 18, 2022, and March 15, 2022. The Ward 32 office was also active participants at public meetings and adjacent resident meetings. Paul Sajovec represented Alderman Waguespack's office at these meetings. These meetings were held to introduce the Project, allow time for the alderman's office to ask questions and share concerns and/or suggestions, discuss public outreach efforts, and share Project updates. Additional meetings with Ward 32 are anticipated during the design process and ahead of construction.
- Ward 47 Summary – Coordination meetings were conducted with representatives from Ward 47 alderman's office on August 10, 2021, January 18, 2022, and March 28, 2022. The Ward 47 office was also active participants at public meetings and community group stakeholder meetings. Coordination meetings were attended by Alderman Martin, Josh Mark, and/or Michael Sewall. These meetings were held to introduce the Project, allow time

for the alderman’s office to ask questions and share concerns and/or suggestions, discuss public outreach efforts, and share Project updates. Additional meetings with Ward 47 anticipated during the design process and ahead of construction.

3.2 Community Groups

Metra held two virtual small-group stakeholder meetings in September 2021 with the Lakeview Roscoe Village Chamber of Commerce and the Roscoe Village Neighbors in the project area. A second joint meeting with these groups and the Ward 47 alderman’s office was held on March 17, 2022. These meetings were held to coordinate issues specific to potential impacts and opportunities to restore key community features such as the “Welcome to Roscoe Village” sign currently located on the Roscoe Street bridge.

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Section 4: Adjacent Resident Coordination

This Project includes a unique circumstance wherein the delineation of property lines is not always visible, and some adjacent properties may be encroaching into UP right-of-way, oftentimes utilizing it as part of their backyard or garden area. This Project requires the reconstruction of abutments and construction of new retaining walls near the UP right-of-way boundary, immediately adjacent to private residential property. To complete this work, temporary construction easements may also be required within adjacent resident property. Therefore, it was important to closely work with property owners in targeted areas along the corridor to describe the Project benefits and communicate potential changes within adjacent railroad right-of-way or temporary easement locations.

To accomplish this, an adjacent resident liaison was selected to coordinate and speak directly with adjacent residents. In addition, a series of individual or group meetings were held with Project team members to better inform residents of the proposed changes from the Project.

4.1 Adjacent Resident Liaison

The Metra Project team included a dedicated bilingual adjacent resident liaison, Melody Carvajal, who coordinated and spoke directly with adjacent property owners and residents. This targeted outreach was conducted primarily through individual or small group one-on-one coordination via email, phone, and direct meetings. The liaison helped initiate individual or group meetings with additional Project team members to help residents understand the Project changes. In addition, the liaison also maintained a log of communications with residents and regularly briefed the Project team on key issues and concerns, potential mitigation measures, and other next steps as the Project moved forward.

Adjacent residents were encouraged to contact the adjacent resident liaison via the website, email updates, and during each of the public meetings.

4.2 Adjacent Resident Meetings

Individual and group meetings were arranged with Project team members for adjacent residents that requested more detailed information about Project components and how it would affect their property. Representatives from UP, Metra, the design team, the environmental review team, and the public outreach team were made available for these meetings. When applicable, group meetings were held with homeowner's associations or other groups of residents who may experience similar changes from the Project. In most cases, the Project team presented details of the Project and how it would affect an individual's property. Residents were then encouraged to provide comments or ask questions.

As of May 27, 2022, a total of 21 meetings were held with individuals or small groups. When requested, slide decks and recordings of meetings were made available for residents who could not

attend group meetings. In some situations, follow up onsite meetings were held to further discuss Project details and constraints that may exist on adjacent properties.

In many cases, individual homeowner's associations west of the existing railroad were targeted and contacted for a meeting due to unique conditions. The adjacent resident liaison also recommended meeting with individuals or groups based on prior contact. In addition, individual meetings with adjacent residents were offered via Project emails and on the Project website in early 2022 and for individuals that specifically requested a meeting during the Project duration.

Section 5: Public Meetings

5.1 Public Meeting #1

The first Public Meeting was held virtually via Zoom on September 9, 2021, from 6:00 pm to 7:00 pm, to introduce the Project. The virtual meeting included a summary of the proposed Project and a 20-minute Q&A session to allow the public to comment and ask any questions. Meeting participants were encouraged to submit questions and comments through Zoom's built-in chat and Q&A boxes.

A total of 145 people registered online for the virtual meeting and 80 were in attendance. The meeting was recorded and uploaded to YouTube and the Project website. An American Sign Language interpreter was present to provide live translation and a recorded version with Spanish subtitles was uploaded to the Project website following translation.

The 45-minute presentation covered the following aspects of the Project:

- Summary of Project components, including anticipated improvements and benefits
- Phases of planned construction work
- Overview of the environmental review process
- A discussion on the cost and funding for the Project
- Project schedule
- Anticipated construction impacts
- Anticipated disruptions to adjacent properties
- Next steps in the Project and public outreach opportunities

5.1.1 Public Meeting #1 Notifications

Metra utilized several mediums to provide public meeting notifications. These include:

- Press Release – A press release was developed to announce the Project and the virtual public meeting.
- Newspaper Advertisements – Newspaper advertisements were placed in Inside Booster, a hyperlocal newspaper, and the Chicago Sun-Times to promote the first public meeting.
- Social Media Content and Partner Kits – Meeting flyers and Project information were provided to partners (Aldermen, stakeholders, etc.) to promote the first virtual public meetings and Project information via their listservs, social media, and websites.

- Flyer – A meeting flyer was posted in English and Spanish at the Ravenswood and Clybourn Metra stations for the first public meeting.
- Yard Signs – Yard signs were developed and placed along the corridor to promote the Project, including the Project website and contact information. The yard signs directed residents to the website for public meeting and detailed Project information. They were placed primarily near the proposed bridge replacement locations along the corridor and at the Ravenswood and Clybourn Metra stations.

5.1.2 Public Meeting #1 Materials

Metra developed a presentation deck that was used virtually for public meeting #1. It was developed and presented in English. The virtual meeting was recorded and posted on the Project website along with a version with Spanish subtitles.

- [Recording of September 9, 2021, Virtual Public Meeting #1](#)
- [Recording of September 9, 2021, Virtual Public Meeting #1 with Spanish Subtitles](#)

5.1.3 Public Meeting #1 Media Coverage

Following the press release announcement and the public meeting, seven outlets shared news about the Project across their various media channels, including television, print, online and social media, totaling 21 media placements. These include:

- ABC Chicago (WLS-TV) — August 24, 2021
- [Progressive Railroading](#) — August 25, 2021
- [Railway Track & Structures](#) — August 25, 2021
- [Block Club Chicago](#) — September 8, 2021
- Inside Booster — September 8, 2021
- [Streetsblog Chicago](#) — September 8, 2021
- FOX Chicago (WFLD-TV) — September 9, 2021
- [Streetsblog Chicago](#) — September 13, 2021

5.1.4 Public Meeting #1 Comments

Following the presentation, the presenters held a 15-minute Q&A session to answer questions asked by the public. Registrants had the opportunity to provide questions during registration and post them live in the Zoom chat box. A list of commonly asked question themes is provided below. A full list of questions can be found in **Appendix B**.

- Clarification and parameters of track shifting west
- Construction timing and potential detours
- Environmental issues including noise/vibration, air quality
- Trees/landscaping/garden impacts
- Community features
- Private property potential impacts

Following Public Meeting #1, an FAQ page was added to the Project website on October 8, 2021, to address some of the most common questions and comments that were received to date. The FAQ page is further described in **Section 0**. In order to address more specific questions and concerns, residents adjacent to the corridor were encouraged to contact the Project's adjacent resident liaison with questions or setup small group or individual meetings. This process is further described in **0**.

5.2 Public Meeting #2

The second Public Meeting was hosted in-person on April 27, 2022, from 6:00 pm to 8:00 pm at Burley Elementary School (1630 W. Barry Avenue, Chicago, Illinois). The public meeting was hosted in an open house format to accommodate differing schedules and provide attendees with the opportunity to learn about components of the Project that they were most interested in. To be responsive to attendee's requests for a group question and answer format, Project staff answered questions as a panel.

A total of 101 people signed in at the registration table and approximately 15 additional people attended but did not sign in. At the registration table, attendees were given a brochure that included an overview of all details presented at the open house as well as a one-page document that provided details regarding the construction options considered. Guests were also given comment cards and stickers to provide comments.

The open house layout included various themed stations with informational boards and dedicated staff to help answer questions and further discuss the Project. The following stations were used at the open house:

- Project Overview – introduced and defined the Project, as well as the need, purpose, and benefits of the Project.
- Construction Overview – introduced the proposed construction plan and renderings and identifies anticipated community construction impacts and changes.
- NEPA Overview – introduced the NEPA process and shared results from the Section 106 review and the noise and vibration analyses.

- Community Voices – offered community members the opportunity to provide feedback and input regarding retaining wall finish, fencing, and vegetation in the public way.
- Recorded Video Presentation – the recorded presentation offered attendees the opportunity to learn about all the information provided at the stations in a video format.

5.2.1 Public Meeting #2 Notifications

Metra utilized several mediums to provide public meeting notifications and encourage individuals to attend the public open house. These include:

- Press Release – A press release was developed for the second public meeting and will be developed for any future public meetings and sent to Metra’s media list before meeting dates.
- Postcard Mailers - Over 9,000 postcard mailers were distributed to residences and businesses within the Project footprint via 15 USPS routes – four Lincoln Park routes, nine Graceland routes and two Lakeview routes. The mailers introduced the Project and encouraged attendance at the second public meeting.
- Email Notifications – Email notifications and reminders were sent to the email list that includes all registration attendees from Public Meeting 1, local stakeholders, organizations, and community groups, and adjacent property owners that has been in contact with the adjacent property liaison. A post-meeting email was also sent out to share all meeting materials with the community, and a comments submission reminder email was sent to encourage feedback from the community. These include:
 - Friday, March 25, 2022 – Save the Date email was distributed to 188 recipients
 - Wednesday, April 6, 2022 – Open House Invite email was distributed to 210 recipients
 - Wednesday, April 20, 2022 – Week Before Reminder email was distributed to 209 recipients
 - Tuesday, April 26, 2022 – Day Before Reminder email was distributed to 202 recipients
 - Wednesday, April 27, 2022 – Day-of Reminder email was distributed to 202 recipients
 - Thursday, May 5, 2022 – Post-Open House email was distributed to 310 recipients
 - Tuesday, May 17, 2022 – Comment Reminder email was distributed to 310 recipients

- Newspaper Advertisement – A newspaper ad was placed in Inside Booster, a hyperlocal newspaper, as well as the Chicago Sun-Times to promote the first public meeting.
- Social Media Content and Partner Kits – Meeting flyers and Project information were provided for partners (Aldermen, stakeholders, etc.) to promote the open house and Project information via their listservs, social media, and websites. The following list is who the partner kit was distributed to
 - Alderman Matt Martin, Ward 47
 - Alderman Scott Waguespack, Ward 32
 - Lakeview/Roscoe Village Chamber of Commerce
 - Greater Ravenswood Chamber of Commerce
 - Roscoe Village Neighbors
 - West Lakeview Neighbors
 - Congressman Mike Quigley, IL District 5
- Flyer – A meeting flyer was be posted in English and Spanish at the Ravenswood and Clybourn Metra stations for the second public meeting.
- Yard Signs – Yard signs were reprinted and placed along the corridor to promote the Project, including Project website and contact information. They direct residents to the website for public meeting and detailed Project information.

5.2.2 Public Meeting #2 Materials

For the open house format for public meeting #2, Metra developed various exhibit boards to present Project information in a series of stations: Project Overview, Construction Overview, NEPA Overview, and Community Voices. At the Community voices station, meeting attendees could provide feedback on retaining wall finish, fencing, and vegetation in the public way. Furthermore, each attendee was provided with a brochure with Project information, a one-pager about the construction options that were considered, and a comment card to provide further input on the Project. Lastly, a recorded presentation was developed and played during the presentation. This recording and a version with Spanish subtitles were posted to the Project website after the meeting.

- [Project Overview Exhibit Boards](#)
- [Construction Overview Exhibit Boards](#)
- [NEPA Overview Exhibit Boards](#)

- [Community Voices Exhibit Boards](#)
- [Brochure](#)
- [Construction Options Considered One-Pager](#)
- [Presentation](#)
- [Presentation with Spanish Subtitles](#)

5.2.3 Public Meeting #2 Media Coverage

Following the press release announcement and the public meeting, seven outlets shared news about the Project across their various media channels, including television, print, online and social media, totaling 22 media placements.

- Inside Booster – April 13, 2022
- WMAQ-TV (NBC) – April 27, 2022
- [Block Club Chicago](#) – April 27, 2022
- WBBM-AM – April 27, 2022 – a total of five on-air mentions and stories aired throughout the day during news segments
- WBBM-TV (CBS Chicago) – April 27, 2022, and April 28, 2022
- [Chicago Tribune](#) – April 28, 2022
- [Streetsblog Chicago](#) – May 2, 2022

5.2.4 Public Meeting #2 Comments

Attendees were able to ask questions and provide comments as part of the event registration, on official comment cards at the meeting, during a group forum that took place during the event, or via email. Individual comments and emails received in connection with the meeting are also included in comments received. A table that lists all comments received can be found in **Appendix C**.

The following table includes a list of common question and comment topic areas. The top areas of concern include the track shift, noise and vibration, environmental, and vegetation and landscaping.

Table 5-1: Public Meeting #2 Count of Questions and Comments by Topic

Common Question/Comment Topic Areas
Railroad Tracks Shift
Noise and Vibration
Environmental Concerns
Vegetation and Landscaping

Common Question/Comment Topic Areas
Adjacent Properties
Retaining Walls
General Project Information
Community Features
Public Meetings
Environmental Review
Bridges and Underpasses
Traffic, Bicycle, and Pedestrian Management
Public Involvement
Construction Timeline
Metra Train Operations
Street Closures and Parking

5.2.5 Public Meeting #2 Public Input

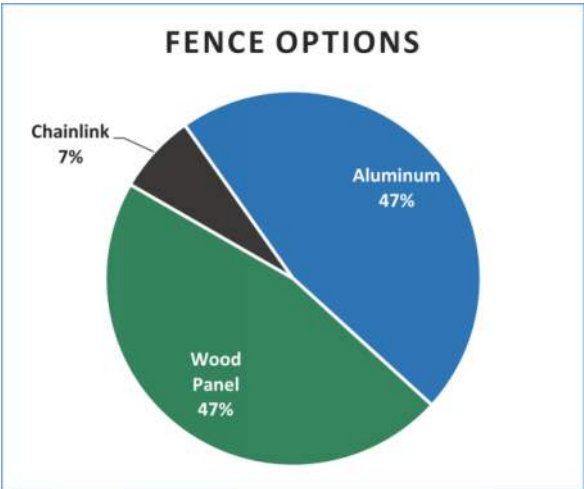
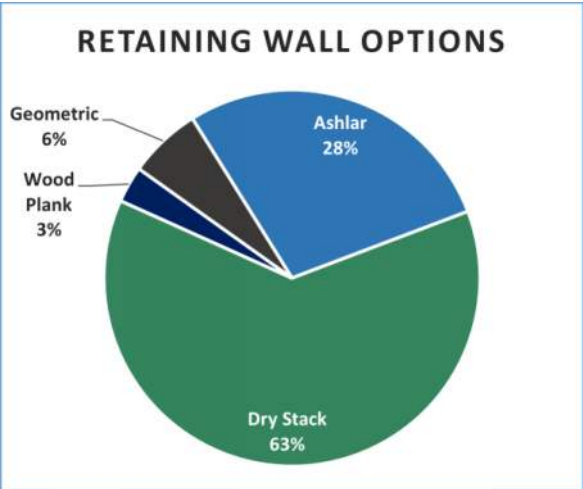
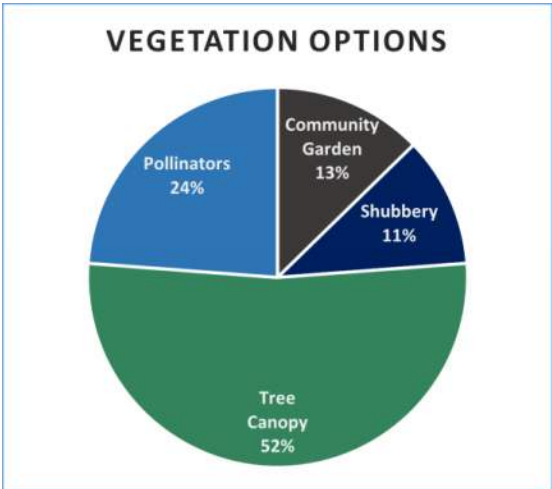
In addition to general public comments and questions, open house attendees were encouraged to provide input on specific design aspects and aesthetic features at the Community Voices station. Attendees were asked to provide feedback on options for potential treatment patterns of new retaining walls, fencing options to be used near the railroad property line, and landscaping options to be incorporated on public way areas adjacent to the railroad corridor following construction. Visualizations were presented to help attendees select options and attendees’ input is being used to help guide the final design.

For those that could not attend or did not provide input at the open house, a Google survey form was developed. This reflected the Community Voices boards from the open house and encouraged additional participation. Design comments were encouraged to be filled out online and submitted by May 23, 2022, to be incorporated into the environmental review process and design considerations. This form was advertised on the Project website and all emails following the open house.

The following are the results of community input on aesthetic options from in-person feedback at the public meeting and through the online survey:

Table 5-2: Results of Public Feedback for Design Considerations

Design Considerations	Options	Results
Vegetation Options	Tree Canopy	33
	Pollinators	15
	Shrubbery	7
	Community Gardens	8
Retaining Wall Treatment Options	Dry Stack	40
	Ashlar	18
	Geometric	4
	Wood Plank	2
Fence Options	Wood Panel	27
	Aluminum	27
	Chain-link	4



Appendices

Appendix A: Public Meeting #1 Summary and Comments

The following includes the list of questions and responses that were addressed live during the Q&A session. Questions not addressed during the Q&A session have been addressed in the FAQ document posted to the Project website and sent to meeting attendees.

How long will temporary construction last?

Temporary access would be needed at some properties immediately abutting UP property. This would primarily occur on the westside but may also occur on the eastside. The construction period would last five years, and temporary access at individual locations would only be required for small portions of this timeline. A further discussion of construction activities, phases of activities, and timing would be discussed at the second public meeting.

Will there be an addition of a Metra station along Project corridor?

The current Project includes bridge replacements and refurbishments, track realignment, and retaining wall replacements. A new station along the Project corridor is not being considered. The closest Metra stations to the Project include Clybourn to the south and Ravenswood to the north.

What will the detour routes and timing for streets bikes and pedestrians be?

The Project is very early in the planning process. Part of design development is identifying detours for automobiles, buses, bikes, and pedestrians. At this time, detailed detour routes and timelines have not been determined. Based on similar work, some closures would be needed from removal and replacement of bridges. Further information on road closures and the timing of construction would be provided and detailed at future public meetings. Metra would work to minimize traffic and pedestrian disruptions to the extent feasible.

How long will construction last at each bridge?

It has not been determined how long each bridge would be under construction. There would be two main phases of work. Bridges would be placed in different stages. There would be several different points of impact, including demolition and shifting of the tracks. It's too early to tell but overall duration is 2023 to 2027. Those stages would occur at different time.

Will work begin in the north and then south?

Where construction begins and which bridge are first worked on would be finalized in design development. Work on west side bridges and tracks would occur first and then phases of the Project would move to the east to reconstruct bridges and tracks.

Will any underpasses be widened for pedestrians and bicyclists?

This would be determined through design development. Roadways have certain right-of-way or public space that they must conform to, most are 66' wide. That's also about the distance between bridge abutments at these locations. There is likely not much room to widen roadways in most cases. There are some restrictions from east to west that are important to consider as well.

Will these improve capacity along the line because of greater speed and efficiency?

Increased train services are not proposed as part of this Project. The track infrastructure is being designed to meet existing track speed limits. These improvements would help avoid the need for slow zones and maintenance delays to create a more consistent track.

Will access to CTA Brown Line be maintained and will the Roscoe Street bridge be repainted?

There would be full plan to maintain access for pedestrians and road users when street closures are needed. An alternative route would be determined, especially for pedestrians getting to brown line. Roscoe bridge has to be removed. We are discussing opportunities for new community identifiers that could be installed. Metra and UP are currently working with the Lakeview Roscoe Village Chamber of Commerce and Roscoe Village Neighbors with what can be completed there.

Will there be something under tracks to catch water or other stormwater drainage system to be installed.

Certainly, the new bridges' style would be different than the current bridges. They would be designed with waterproofing in mind and help drain stormwater. These are larger bridge decks would help divert stormwater as well.

Will lowering the roadways cause flooding?

The design team is finalizing the Project components, but they would likely need to lower some roadways six to twelve inches. As part of the design process, the design team and relevant agencies would make sure appropriate stormwater management is in place and flooding would not be a concern. Some of the existing bridges did previously require lowering roadways. It's a similar approach that has been done before. Near the Brown Line additional space is needed to provide clearance.

Were there any lessons learned from previous phase?

Yes, many lessons were learned. One that has been very important to Metra is to keep two tracks open for active train operations at all times during construction.

Will there be equipment in the neighborhood?

Metra is not at construction period yet, that would be 2023. Some construction disruptions would be expected though. You can expect some temporary street closures, detours, and construction workers and equipment within the area. The extent is currently not finalized. There would be some construction related parking that may be identified away from the corridor. Metra would work with the contractor for some preidentified locations for parking of construction workers.

Will there be an assessment on air pollution and air quality in the environmental review?

There would be a discussion on air quality conformity in the DCE document. That would be posted to Project website when ready.

Can the noise and vibrations documents be shared?

These documents are currently being developed and results would be shared when finalized.

People have seen trucks hitting the bridges. Will clearance be added as part of the new bridges?

New bridges would maintain the existing clearance or be raised slightly. One aspect of bridge design would include a sacrificial beam. This is added to protect the bridge from vehicle strikes so bridges would not be damaged.

Will the tracks be built to allow for heavier trains?

There is a large Metra class of trains that could run on this track after construction.

Will there be an increase in train service?

There would be no increase in train services as part of Project. The improvements would help meet existing speed limits on the tracks.

How far west will the tracks be moved?

Design is still in development. The key design factor is to make sure there is enough room to maintain the two tracks of service during construction periods. The team is looking at a variety of distances as part of the Noise and Vibration studies and incorporates those distances into the modeling. It is important to reiterate that all improvements would occur within existing UP property. There would be no permanent acquisition or impacts to buildings by this work. Some temporary access may be needed and some locations where a private fence on UP right-of-way and that configuration would change.

Some questions were asked on specific adjacent properties such as disruptions to views, specific fences, or other circumstances.

Individual adjacent residents should reach out to Melody Carvajal where one on one or group coordination can occur. There are many different circumstances along the corridor, and it is best to discuss specific concerns with individuals.

Are there renderings available?

This is the start of Project and there are no renderings yet. These would likely be created and are planned to be shared at the second public meeting. More information and visuals to help communicate how the Project would look are anticipated then.

For anyone who registered though Zoom and asked for Project updates, their email address has been added to the distribution list. Email blasts would be made about Project updates and schedule of the next public meeting.

Will there be only two public meetings?

There is some opportunity to reach out to individuals about specific circumstances and if there is a need for a 3rd public meeting, which can be accomplished. That would be based on Project schedule and how it moves forward. For any individual who live adjacent to the UP property, please contact Melody Carvajal. It is important for residents to you know what's coming and for the Project team to hear any concerns about the Project.

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Q+A	Hi will community gardens be built along the tracks?
Q+A	Will lighting improve under the bridges? Many have inadequate lighting.
Q+A	1) Where is funding for this coming from? 2) What plans are in place so this does not affect street parking for residents? 3) What are the plans for reducing noise? As many of us have transitioned to working from home during this past year, it is discourteous to make large noise during the working day (and the night too, of course)
Q+A	Will there be something under the tracks / over the sidewalks at each bridge to catch water that drips down after rain / snow?
Q+A	where and by how much will the tracks be shifted west? Will neighborhood groups have any input to the design?
Q+A	Will the shift in track realignment increase noise or vibration west of the tracks? Will the landscaping and tree screen along the tracks be replaced in any way?
Q+A	How far west will it be shifted? Will Ravenswood street be impacted?
Q+A	Is lowering the streets a good idea considering flooding risks, especially in light of global warning?
Q+A	How much will Roscoe Street be lowered and what will that look like?
Q+A	exactly which portions of track are being shifted west and how far west are they being shifted? I live on the west side of the tracks on N Ravenswood between Belmont and Melrose. Interested in finding out how much closer the tracks will be to my house.
Q+A	How many feet do you to shift the tracks West? And how do you balance the impact to homeowners who live adjacent to the West side of the rail line?
Q+A	Replacing 12 bridges in 4 years, one every four months. How realistic is that? (Looking at e.g., the Circle Exchange)
Q+A	Will the walls be attractive like they are north of Grace
Q+A	sure! my email is [individual email]
Q+A	What steps will be taken to preserve aesthetic features in the corridor, such as the painted girders at Roscoe St?
Q+A	I am concerned with the Metra UP-N line's excessive air (diesel exhaust) and noise pollution going through some of Chicago's most populated neighborhoods. Is electrification part of this plan?
Q+A	What defines an adverse event? Thank you.
Q+A	does anyone in the public actually want this project to happen, or is this just the Metra's desires?

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Q+A	How much will the green spaces along the tracks be impacted? What does it mean that they'll be impacted? Will they be removed and replaced with the industrial looking retaining walls and tracks?
Q+A	Will a new retaining wall be installed by Metra , and how far to the West of where it is currently will the new wall be at Diversey?
Q+A	Will you be restoring landscaping similar to what it is currently after the construction ends, at all locations and specifically at Diversey?
Q+A	What is the timeline for each individual bridge start to finish, so we can each see timing for impact based on where we live?
Q+A	I've seen trucks hitting the bridge at Belmont often. Will clearance be added to prevent this or is this a result of settling in the retaining wall.
Q+A	Will the old retaining wall blocks be removed? I've seen blocks from the completed bridges still lining Ravenswood Ave north of Lawrence.
Q+A	For residents who have their houses close (1800 diversely area) to the tracks, with the movement of the tracks will there still be a safe distance to our houses ...
Q+A	How many feet to the West of where it is currently will the West side of the new setup be... e.g., the new wall be at Diversey?
Q+A	will street be paved after construction?
Q+A	Understanding you're still working on the next phase... How far to the West were the bridges completed to date moved?
Q+A	near Wellington on Ravenswood there is a worker's staircase. Can that staircase be better secured so pedestrians do not walk up on the tracks?
Q+A	How can we determine where the property line, indicating the edge of Metra-owned property, is?
Q+A	Will these slides and recording be made publicly available?
Chatbox	What portion of the track will be moving West and how far? What will the new retaining wall look like? Trees? Will Equipment be in our yard for houses directly on the track line? ETC
Chatbox	How far will you be shifting west??? Feet? Inches?
Chatbox	I do have concerns about lowering streets and having increased flooding in these underpasses after rainstorms
Chatbox	Do you use graffiti-resistant materials?
Chatbox	How will the shifting of the tracks to the West impact homeowners who live adjacent to the Metra Line?
Chatbox	exactly which portions of track are being shifted west and how far west are they being shifted? I live on the west side of the tracks on N Ravenswood between Belmont and Melrose. Interested in finding out how much closer the tracks will be to my house.

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Chatbox	I also have concerns about street flooding. What storm water management practice will be implemented to combat this?
Chatbox	The new track will bring the train closer to homes on the west side. How much further west will the tracks shift, specifically? My question is specifically at Barry Ave.
Chatbox	As the retaining walls are rebuilt, what are the updated O&M plants for vegetation? The bridge at Belmont and Ravenswood has a lot of overgrown invasive species which impede pedestrian visibility, particularly for people walking eastbound on the south sidewalk on Belmont.
Chatbox	Why not keep trains to the east where there are no homes??
Chatbox	would really like the trees re-planted because it helps reduce the noise
Chatbox	And smell/pollution
Chatbox	Up north rebuild was all public road. South of Belmont on the west is private property - backyards and private alleys. How will this be handled in the rebuild especially the retaining walls.
Chatbox	yes, and minimize how many trees are removed in the first place. is it possible to keep some existing trees?
Chatbox	what about long-term control of the proliferation of weeds, weed trees, and control of the spaces alongside the tracks?
Chatbox	I agree, Tim. The vegetation helps keep the tracks from looking to industrial. I'm concerned about losing them and how that will impact the neighborhood.
Chatbox	will you share the noise & vibration data?
Chatbox	Can you please provide a link to the noise and vibration study?
Chatbox	Yes, my condo looks out onto the tracks. worried about property valuation if the nice vegetation is removed. I certainly wouldn't have paid as much for this place if there weren't trees blocking the tracks
Chatbox	Seems as if there is an entire new track to be laid down west of the unused 3rd bay. What is the width of the new track? Currently we have a wooden fence protecting our children and pets from your tracks. You said retaining walls to be replaced, will you be putting a barrier wall, a wooden fence, or a chain fence like the east side has?
Chatbox	Having recently completed 11 new bridges in the Ravenswood/Andersonville area, what were the key takeaways from that project that are applicable to this project further south? What did Metra and its contractors learn? What would they have done differently? Was this completed project finished on time? What surprises or unexpected challenges did Metra face -- and how were they resolved?

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Chatbox	Will the new track allow Metra to buy newer trains with less pollution instead of what you did last time to get around EPA rules which was buy old grandfathered Amtrak trains that spew diesel exhaust?
Chatbox	how much will this project save long term since the bridges are fine and it's the maintenance that's expensive? What's the return of investment on the 261 million?
Chatbox	in a meeting with Alderman Waguespack on Tuesday the City of Chicago is removing more trees than replanting. Please re-plant trees to assist with the environmental impact and noise element
Chatbox	Our first row of Townhouses are not 20 feet away. Bringing trains further west does nothing but lower our values. Why not keep it where it is and improve the bridges and supports. Add more trees not take away those there.
Chatbox	This is a very quick timeframe. And with a nearly \$95 million gap in funding, how can you confidently think this will be achieved in the next 6 years
Chatbox	It sounds like moving diesel locomotives closer to residences while removing vegetation is not a healthy idea if electrification is not happening at the same time? Also have not heard air pollution impact mentioned as part of the environmental review.
Chatbox	Are the tracks being built to allow for heavier trains- or existing weights on trains? Is that reason for bridges so you can manage heavier cars on same track? Why leave the phantom third track to be built in future? Can we expect increase in train service once complete?
Chatbox	Why is it necessary to shift the tracks west? This seems like a huge disturbance to the population that live adjacent to the Metra tracks
Chatbox	Agreed! I don't understand who actually wants this project to happen
Chatbox	If anyone would like to coordinate as people who live adjacent to the tracks on the west, email me and I'll set up a listserv for us to chat. You can email me at lsnyder@uniformlaws.org -- perhaps we can organize our comments and concerns
Chatbox	If the tracks don't shift, Metra couldn't maintain both tracks for active service. They have to build a new track adjacent to the existing ones so service is maintained.
Chatbox	Yes why can't the tracks be shifted east instead? No properties abut this area.
Chatbox	What does that mean that the features will be "impacted"? Will those areas now be demolished and replaced with tracks?
Chatbox	My property is on the UP railroad right of way. The purchase price reflected that fact and it was abundantly obvious both when we first saw the house, when we received the plat, and when the property lines were marked (required by law). Also, I'm not paying property tax on the UP

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
	owned land that we've been using so our tax bill is lower than our neighbors who aren't using the UP land. No complaints here.
Chatbox	You are keeping the Addison St bridge but moving the tracks as described within the existing bridge structure, right?
Chatbox	We have a lot of new landscaping we just completed within our property. Yet this is adjacent to the retaining wall. It would seem this might be damaged/destroyed for this project. Will we be compensated for new landscaping?
Chatbox	Do you have a list of which adjacent properties will need construction access?
Chatbox	What is the construction duration for each individual location?
Chatbox	I'm hopeful that bright security lights under the tracks will be attractive, energy efficient? And, will cellular wireless boosters be placed along the tracks? if so, will they be attractive?
Chatbox	Is upnrebuild@metrarr.com correct. Two rr after Metra?
Chatbox	will there be "just compensation" for Metra taking partial or temporary use of private property
Chatbox	Will there be community gardens planned along the tracks and overall beautification of the environment? The tracks north of Grace are much more attractive and involve a lot of community gardens
Chatbox	Is Metra going to pay for lost parking spots and fees? How temporary is temporary?
Chatbox	I'd like to see diagrams from above that actually reflect how property line encroachment will occur. The perspective diagrams don't really tell the story.
Chatbox	I'm not impacted by this project - we live well north of it. Just giving my feedback for those in my boat. :-)
Chatbox	Also, will fencing be moved? If so, will old fencing be replaced?
Chatbox	How many feet west will it be moved?
Chatbox	I agree. also, the increased noise pollution may raise to the level of inverse condemnation, especially if all of the trees that mitigate noise are ripped out and the tracks are moved west
Chatbox	Thanks Metra staff for hosting the meeting, my apologies for arriving late. Look forward to working with your staff on these bridge/rail projects, and we will be open to future meetings or discussions on any concerns with neighbors as we move forward. Thanks again for hosting. Ald. Waguespack
Chatbox	will the heavier trains be louder?
Chatbox	How many feet west will the tracks be moved? And how close to property line is the track allowed to be safely?

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Chatbox	Will the green spaces along west Ravenswood be removed and replaced with retaining walls? Do you have any mock-ups of what this will look like?
Chatbox	will there only be two public meetings? or will there be additional meetings after the second one?
Chatbox	Ryan, I was wondering the same. I really like the green spaces.
Chatbox	What. Melanie's email restate please.
Registration	Will these improvements at all help increase capacity of the line because of greater speeds and efficiency?
Registration	For those of us who live in homes on Honore St, how fare west are the tracks being shifted? What will reduce vibrations/noise?
Registration	Any station to be added between Ridge & Peterson?
Registration	The eastern embankment from Belmont to Roscoe is unsightly. What is planned to improve the appearance?
Registration	Will personal property structures (i.e.: garages) be impacted? Where will walls be?
Registration	What can we expect in terms of views and trees east of the tracks between Barry and Belmont?
Registration	I'd like to speak with someone about the impact at our home - our yard borders the train directly. I'm addition, we utilize our
Registration	How is this going to impact homeowners with backyards along the tracks?
Registration	When would the work at Ravenswood and Barry commence?
Registration	How long is construction expected to last at each bridge (i.e., Diversey)? How long homeowners will be directly impacted?
Registration	Has the IL State Historic Preservation Office determined any of the bridges as eligible for listing in the National Register?
Registration	Is this project truly necessary?
Registration	When and What work will be done from Diversey to Wellington
Registration	Will the properties along the tracks be impacted by "Shifting tracks west to align with the existing tracks"?
Registration	My property backs up to the west side of the Metra tracks; how will this affect my property?
Registration	Will there be any noise/vibration level assessments or precautions performed/employed in the urban areas?
Registration	1. What is the traffic management plan on Ravenswood going to look like during construction?
Registration	Will stormwater management practices be installed? Will O&M be updated to cut back invasives and improve ped visibility?

Public Meeting #1 All Questions and Comments	
Question Source	Questions/Comments
Registration	What are the plans to electrify the UP-N line and are the bridge designs able to accommodate this?
Registration	Will Metra change to trains within EPA regulations instead of spewing out diesel from old trains for my kid to breathe?
Registration	What are the parameters for "shifting the tracks west"? What does this mean? How will I be impacted by this work?
Registration	Hi. 1) Access to the Brown line from Roscoe Village of Roscoe is closed for lowering? 2) Will the Roscoe bridge be repainted?
Registration	Timeline for start? Hours/days for work on School/Ravenswood bridge? (Considering remote working)
Registration	Will work start on the north and move south? Are there renderings of the west retaining wall Addison to Belmont?
Registration	Will trees be removed/ replaced
Registration	Will any underpasses be widened or narrowed to accommodate wider sidewalks, bike lanes, or reduced vehicle lanes?
Registration	2025 W Potomac Avenue
Registration	Will you record and make this session available? Just found out about it an hour ago, and I have a previous obligation. Thanks.
Registration	Are there any plans to add any new stations?
Registration	Will there be funding and/or plans for community gardens and beautification of the tracks such as was done farther north?

2) Chicago Sun-Times advertisement

3) Flyers at the Ravenswood Metra station

4) Yards signs in the community



5) Social media announcement and shares

Metra UP-N
7,601 Tweets

Metra UP-N
@metraUPN

Metra provides service to and from downtown Chicago with 242 stations over 11 routes totaling nearly 600 route miles & approx. 1,200 miles of track.

Chicago, IL metra.com Joined February 2009

0 Following 4,061 Followers

Metra UP-N Retweeted

Metra @Metra · 17h
Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago. Read more: bit.ly/2WpTGsN

UP NORTH REBUILD
FULLERTON TO ADDISON

VIRTUAL PUBLIC MEETING

7 6

Metra
August 24 at 1:29 PM · 🌐

Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago. Read more: <https://bit.ly/2WpTGsN>

UP NORTH REBUILD
FULLERTON TO ADDISON

VIRTUAL PUBLIC MEETING
SEPTEMBER 9 / 6:00 P.M. TO 7:00 P.M.

REGISTER TODAY ON ZOOM!

25 3 Shares

Roscoe Village Neighbors
Yesterday at 1:51 PM · 🌐

Metra is in the beginning phases of a rebuild of the Union Pacific North (UP-N) Line. This will include replacement of 11 bridges between Fullerton to Cornelia as well as significant replacement of retaining walls from Fullerton to Addison. A virtual public meeting will be held on Thursday, September 9th from 6pm to 7pm. RVN encourages all interested neighbors to attend. More information is available at the Metra website here (<https://metra.com/UPNrebuild>). To register for the virtual public meeting on September 9th, please click here (https://zoom.us/j/.../register/WN_xCr3UKP5QnO6ATgkFUuNA%20)

UP NORTH REBUILD
FULLERTON TO ADDISON

VIRTUAL PUBLIC MEETING
SEPTEMBER 9 / 6:00 P.M. TO 7:00 P.M.

REGISTER TODAY ON ZOOM!

Metra
September 2 at 9:10 AM · 🌐

UP-N Riders: Metra will hold a virtual public meeting on Sept. 9 from 6 p.m. to 7 p.m. to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago. Learn more: <https://bit.ly/2WpTGsN>

1 Comment

Lakeview Roscoe Village Chamber of Commerce
September 7 at 3:01 PM · 🌐

A new project is coming to our community - Metra will be modernizing our 120-year old bridges along the UP-N Line! Join Metra virtually for the Project's first public meeting on September 9. Register here: https://zoom.us/.../register/WN_xCr3UKP5QnO6ATgkFUuNA%20/

UP NORTH REBUILD
FULLERTON TO ADDISON

VIRTUAL PUBLIC MEETING
SEPTEMBER 9 / 6:00 P.M. TO 7:00 P.M.

REGISTER TODAY ON ZOOM!

1 Share



6) Aldermanic e-newsletter



**Metra Bridge Replacement
Addison to Fullerton**

Metra is hosting a virtual public meeting on Thursday, Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line. This phase includes the replacement of 11 bridges and retaining walls as required between Fullerton and Addison.

The Metra UP North Rebuild: Fullerton to Addison Project is currently in the design and planning stage with construction expected to begin in 2023 and completion expected in 2027.

To introduce the project to adjacent communities, Metra will be holding a virtual public meeting on Thursday, Sept. 9 from 6 p.m. to 7 p.m. Meeting attendees will hear about Metra plans to modernize the bridges and track structures and learn additional details about the project's timeline and benefits. [Register here.](#)

Additional project elements include shifting the tracks west within the existing railroad right-of-way to align with the tracks north and south of the project area; reconstructing underpasses with increased lighting and ADA accessible travel paths; lowering of Roscoe and Cornelia to maintain current clearance under the UP North Line and the CTA Brown Line; refurbishing and painting of the existing Lincoln/Addison bridge; and some utility work along the project corridor. [Learn more at metra.com/UPNrebuild.](#)



Public Meeting #1 Media Coverage

1) ABC Chicago (WLS-TV) — August 24, 2021 - local news segment



2) Progressive Railroading — August 25, 2021 - national trade online publication and social media shares



Metra sets meeting to discuss UP North bridge project

Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on Chicago's north side.

The Fullerton-to-Addison portion of the project calls for replacing 11 bridges and retaining walls. The next phase is in the design and planning stage, with construction expected to begin in 2023 and conclude in 2027.

Additional project elements include shifting the tracks west within the existing right of way to align with the tracks north and south of the project area; reconstructing underpasses with increased lighting and ADA accessible travel paths; lowering of Roscoe and Cornelia streets to maintain current clearance under the UP North Line and the CTA Brown Line; refurbishing and painting of the existing Lincoln/Addison bridge; and completing some utility work along the corridor.



"These bridges have surpassed their functional lifespan and can no longer be economically repaired and maintained," said Metra Chief Executive Officer and Executive Director Jim Derwinski in a news release. Photo - Metra

"These bridges have surpassed their functional lifespan and can no longer be economically repaired and maintained," said Metra Chief Executive Officer and Executive Director Jim Derwinski in a news release. "The project will modernize the existing infrastructure, reduce maintenance needs and operating costs, and improve the customer experience with increased reliability and resiliency."

The \$262.3 million project has been included in Metra's capital program for the past three years. Funds totaling \$161.2 million are currently allocated, including \$6 million from federal sources and \$156.1 million from Rebuild Illinois bond funds. The agency continues to pursue additional funding.

Under the project's first phase, Metra replaced bridges over 11 streets from Grace to Balmoral. The agency also is reconstructing Ravenswood Station, a \$166 million project that should be completed next year.



3) Railway Track & Structures — August 25, 2021 - national trade online publication and social media shares

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Metra to hold virtual meeting on UP North bridge project

Written by [David C. Lester, Managing Editor](#) | August 25, 2021

Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago.

This phase includes the replacement of 11 bridges and retaining walls as required between Fullerton and Addison.

Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago. This phase includes the replacement of 11 bridges and retaining walls as required between Fullerton and Addison.

The Metra UP North Rebuild: Fullerton to Addison Project is currently in the design and planning stage with construction expected to begin in 2023 and completion expected in 2027.

"These bridges have surpassed their functional lifespan and can no longer be economically repaired and maintained," said Metra CEO/Executive Director [Jim Derwinski](#). "The Metra UP North Rebuild Project will modernize the existing infrastructure, reduce maintenance needs and operating costs, and improve the customer experience with increased reliability and resiliency. This is a major project that will benefit My Metra riders for decades."

To introduce the project to adjacent communities, Metra will be holding a virtual public meeting on Thursday, Sept. 9 from 6 p.m. to 7 p.m. Meeting attendees will hear about Metra plans to modernize the bridges and track structures and learn additional details about the project's timeline and benefits. [To register for the virtual public meeting click here](#) or to learn more about Metra UP North Rebuild: Fullerton to Addison, [click here](#).

The project will reduce Metra's carbon footprint by improving energy efficiency, reducing oil and diesel fuel dependence and improving air quality, because it will result in fewer service delays and a greater ability to maintain consistent locomotive speeds.

Additional project elements include shifting the tracks west within the existing railroad right-of-way to align with the tracks north and south of the project area; reconstructing underpasses with increased lighting and ADA accessible travel paths; lowering of Roscoe and Cornelia Streets to maintain current clearance under the UP North Line and the CTA Brown Line; refurbishing and painting of the existing Lincoln/Addison bridge; and some utility work along the project corridor.

The project is anticipated to be constructed in two phases to maintain all rail operations and reduce impacts to pedestrian, bicycle, and automobile traffic. There will be some street and sidewalk closures throughout the project and the community will be notified beforehand. Additionally, property owners with backyards, gardens, or other property adjacent to the railroad may be impacted by the construction. Metra will be conducting outreach and coordination with these property owners.

The total estimated cost of the project is \$262.3 million and has been included in Metra's Capital Program for the past three years. Funds totaling \$161.2 million are currently allocated, including \$6 million from federal sources and \$156.1 million from Rebuild Illinois bond funds. Metra continues to pursue additional project funding.

In the first phase of the project, bridges over 11 streets from Grace to Balmoral were replaced. The reconstruction of the Ravenswood Station, which should be complete next year, was also part of that phase which cost \$166 million.

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Metra to hold virtual meeting on UP North bridge project - Railway Track and Structures



Metra to hold virtual meeting on UP North bridge project - Railway Track and Structures. This phase includes the replacement of 11 bridges and retaining walls as required between Fullerton and Addison. rtands.com

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Metra to hold virtual meeting on UP North bridge project - Railway Track and Structures

4) **Block Club Chicago** — September 8, 2021 – local online news publication, e-newsletter, and social media shares

BLOCK CLUB CHICAGO



Metra To Replace 11 Old Bridges Through Lakeview As Part Of Union Pacific North Line Overhaul

Metra is replacing 120-year-old bridges from Fullerton Avenue to Cornelia Street. Neighbors can learn more at a Thursday night virtual meeting.

John Wronch | 12:04 PM CDT on Sep 8, 2021

LAKEVIEW – Metra is announcing plans to replace 11 of 8 bridges through Lakeview over a four-year stretch that will be completed by neighborhood residents Thursday as it prepares to move into a new phase of a \$282.3 million overhaul of the Union Pacific North Line.

The bridge replacements from Fullerton Avenue to Cornelia Street are part of the **UP North** project by Metra to modernize the UP North line. Metra already replaced 11 bridges from Grace Street to Belmont Avenue, and began reconstructing the Park Renaissance Station, 4600 N. Renaissance Ave.

Metra officials will detail the work on the 120-year-old bridges at a virtual community meeting 8 p.m. Thursday. Registration is [QUICK](https://bit.ly/3t0k88z).

"These bridges are old," Metra spokesperson Michael Gilis said. "They're safe to use, but it takes a lot of time and money to keep them that way year by year. They've reached the end of their useful lifespan and need to be replaced."

Replacing the bridges will cut down on service delays by reducing the likelihood of slow zones along the corridor, Gilis said. It will also reduce potential service interruptions due to bridge maintenance needs.

These adjustments will also reduce Metra's carbon footprint by reducing oil and diesel fuel dependence with fewer service delays and greater ability for trains to maintain consistent speeds, he said.

"Also, if we're able to replace the bridges, these costs to keep them up will go down, and we can use that money to maintain the rest of the system," Gilis said.


The project is in its design and planning phase, Gilis said, with construction expected to run in two phases from 2023 to 2027.

Other elements of the project include replacing the railroad's retaining walls where needed from Fullerton to Addison Street, shifting the tracks slightly west, lowering Fullerton and Cornelia streets to maintain current clearance under the CTA Brown Line, rehabilitating and painting the concrete highway bridge, and some utility work along the UP North line.

The project will require some street and sidewalk closures, but neighbors will be notified beforehand, officials said.

Property owners with backyards or gardens next to the railroad might be affected by construction, Gilis said. Metra will identify and reach out to those residents to work with them throughout the project.

The full project is expected to cost \$282.3 million, Metra officials said. So far, \$141.2 million has been allocated, including \$6 million in federal funding and another \$116.1 million from Metra's Illinois bonds.



BLOCK CLUB CHICAGO

Your Daily Dose of Neighborhood News

September 9, 2021



LAKEVIEW
Metra To Replace 11 Old Bridges Through Lakeview As Part Of Union Pacific North Line Overhaul

Metra is replacing 120-year-old bridges from Fullerton Avenue to Cornelia Street. Neighbors can learn more at a Thursday night virtual meeting.

Block Club Chicago 5d · 🌐

Metra is replacing 120-year-old bridges from Fullerton Avenue to Cornelia Street. Neighbors can learn more at a Thursday night virtual meeting.



BLOCKCLUBCHICAGO.ORG

Metra To Replace 11 Old Bridges Through Lakeview As Part Of Union Pacific North Line Overhaul

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Metra is replacing 11 old bridges through Lakeview as part of its Union Pacific North line overhaul. buff.ly/38OdHfi



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5) Inside Booster — September 8, 2021 - hyperlocal print publication

INSIDE PUBLICATIONS SEPTEMBER 8 - SEPTEMBER 14, 2021 • 5

Metra to hold virtual meeting Sept. 9 on UP North \$262M bridge project

Rehab of 11 bridges planned
BY SIBILLA SWANN

Metra bridges on the North Side are about to get a major \$262 million overhaul. Metra will hold a virtual public meeting on Sept. 9 to introduce the next phase of its project to replace 120-year-old bridges along the Union Pacific North Line on the North Side of Chicago. This phase includes the replacement of 11 bridges and retaining walls as required between Fullerton and Addison.

The Metra UP North Rehabilitation Fullerton to Addison Project is currently in the design and planning stage with construction expected to begin in 2023 and completion expected in 2027.

The bridges being targeted are 120 years old. Along with the adjacent retaining walls, these bridges have surpassed their functional lifespan and are no longer be economically repaired and maintained.

Metra operated 78 revenue train cars weekly on the UP-N Line with average weekday ridership of 14,000 passengers. The line runs all the way north to Kenosha, WI.

The total estimated cost of the project is \$262.3 million and has been included in Metra's Capital Program for the past three years. Funds totaling \$161.2 million are currently allocated, including \$6 million from federal sources and \$155.2 million from Rahmâll Illiotin bond funds. Metra continues to pursue additional project funding.

"These bridges have surpassed their functional lifespan and are no longer be economically repaired and maintained," said Metra CEO Executive Director Jim Derwinski. "The Metra UP North Rehabilitation Project will modernize the existing infrastructure, reduce maintenance needs and operating costs, and improve the customer experience with increased reliability and resiliency. This project will benefit Metra



BRIDGE from p. 5

riders for decades."

To introduce the project to adjacent communities, Metra will be holding a virtual public meeting 9 p.m. to 7 p.m. Thursday, Sept. 9. Meeting attendees will hear about Metra plans to modernize the bridges and track structures and learn additional details about the project's timeline and benefits.

To register for the meeting, visit <https://buff.ly/38OdHfi>.

For more information about Metra UP North Rehabilitation, visit <https://metra.com/UP-NorthRehab>.

The project will improve energy efficiency, reducing oil and diesel fuel dependence and improving air quality, because it will result in fewer service delays and a greater ability to maintain consistent locomotive speeds.

Additional project elements include shifting the tracks west within the existing railroad right-of-way to align with the tracks north and south of the project area.

reconstructing underpasses with increased lighting and ADA accessible travel paths; lowering of Racine and Cornelia streets to maintain current clearance under the UP North Line and the CTA Brown Line; refurbishing and painting of the existing Lincoln/Adams bridge; and some utility work along the project corridor.

The project will be constructed in two phases to maintain all rail operations and reduce impacts to pedestrians, bicycle, and automobile traffic. There will be some street and sidewalk closures throughout the project and the community will be notified beforehand. Additionally, property owners with backyards, gardens, or other property adjacent to the railroad may be impacted by the construction. Metra will be conducting outreach and coordination with those property owners.

In the first phase of the project, bridges over 11 miles from Grace to Belmont were replaced. The reconstruction of the Ravenswood Station, which should be complete next year, was also part of that phase which cost \$166 million.

BRIDGE see p. 7



6) **Streetsblog Chicago** — September 8, 2021 – local online real-estate/construction publication and social media shares

STREETSBLOG CHICAGO
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Metra holds virtual meeting on \$262.3M project to replace UP North bridges Thursday
 By John Greenfield | Sep 8, 2021

Tomorrow Metra hosts a virtual public meeting on its upcoming \$262.3 million project to rebuild eleven 120-year-old Union Pacific North line bridges in Lakeview and Lincoln Park. The initiative also includes replacing retaining walls as necessary on the route, which runs between the Ogilvie Center and Kenosha, Wisconsin.

The Metra UP North Rebuild: Fullerton to Addison Project is now in the design and planning stage, with construction slated to start in 2023, wrapping up in 2027. An environmental review process is being conducted in accordance with the National Environmental Policy Act, which also includes a noise and vibration analysis.

"These bridges have surpassed their functional lifespan and can no longer be economically repaired and maintained," said Metra CEO Jim Derwinski in a statement. "The Metra UP North Rebuild Project will modernize the existing infrastructure, reduce maintenance needs and operating costs, and improve the customer experience with increased reliability and resiliency. This is a major project that will benefit Metra riders for decades."

The virtual community meeting takes place on Thursday, September 9, from 6-7 p.m. [Register for the Zoom event here.](#)

Metra says the bridge project will reduce the railroad's carbon footprint by improving energy efficiency, lowering oil and diesel fuel use, and improving air quality, because it will result in fewer service delays and a greater ability to maintain consistent locomotive speeds.




Other aspects of the project include relocating the tracks west within the existing railroad right-of-way to align with the tracks north and south of the project area; rebuilding viaducts with more lighting and wheelchair-accessible routing; lowering Roscoe and Cornelia streets to maintain current clearance under the UP North Line and the CTA Brown Line; refurbishing and painting the existing Lincoln/Addison bridge (which isn't being replaced); and some utility work along the corridor.

An interesting factoid about this portion of the Ravenswood corridor is that when those bridges were originally built, this portion of the UPN line had three tracks. It was reduced to two tracks in the early 1980s, but the bridges still have space for a third track.

According to Metra, the UPN Rebuild is expected to be built in two phases to avoid major rail service disruptions and reduce impacts on other travel modes, although there will be some street and sidewalk closures.

The \$262.3 million project cost has been included in Metra's Capital Program for the past three years. \$161.2 million is currently allocated, including \$6 million from 2019 Federal Transit Authority 5337 State of Good Repair Funds, and \$156.1 million from Rebuild Illinois bond funds. Metra is still identifying the rest of the funding.



The \$166 million first phase of the project replaced 11 bridges from Balmoral Avenue to Grace Street, and also included the reconstruction of the Ravenswood station, which should be finished in 2022.

[More info on the UPN Rebuild is available here.](#)



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There will be a virtual meeting on Metra's plan to replace 11 Union Pacific North bridges in Lakeview and Lincoln Park tonight at 6 PM.




Metra holds virtual meeting on \$262.3M project to replace UP North bridges T... The initiative will rebuild eleven 120-year-old Union Pacific North line bridges in Lakeview and Lincoln Park.
chlstreetsblog.org

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 50

There will be a virtual meeting tomorrow at 6 PM to discuss an upcoming project to rebuild 11 bridges on Metra's Union Pacific North line in Lakeview and Lincoln Park.



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Metra holds virtual meeting on \$262.3M project to replace UP North bridges Thursday

1 Share

7) FOX Chicago (WFLD-TV) — September 9, 2021 - local news segment



8) Streetsblog Chicago — September 13, 2021 - local online real-estate/construction publication and social media shares

STREETSBLOG CHICAGO

More deets about the UPN Line bridge replacement project from recent meeting

By Iqbal Siddiqui on Sep 13, 2021

As Metra and Union Pacific Railroad are getting up to replace [along I-55 east of Union Pacific North Line bridge](#) in Lincoln Park and Lakeview between Addison Street and Fullerton Avenue, nearby residents and businesses are wondering about the construction impacts, including possible flooding issues, and just how close to their homes the new tracks will be.

However, the residents didn't get many answers during Metra's September 9 virtual public meeting. The first of several planning meetings about the project. As Metra officials and consultants emphasized throughout the meeting, the details will be worked out once the railroad completes the federal environmental review and the engineering study. The design isn't expected to be completed until the summer of 2023, with construction taking place between 2023 and 2027. But the officials are expected to share more details at the second public meeting, which will take place either toward the end of 2021 or beginning of 2022.

In the meantime, residents got a chance to look at the plans. The bridge replacements will follow a similar process as earlier bridge replacements on the section of the line in Edgewater and Uptown, between Grace Street and Belmont Avenue. The tracks will be shifted west, and most bridges will be completely replaced, and the embankment retaining walls will be shored up.

The Addison-Caneva Bridge, which was built more recently during the Richard J. Daley administration, will simply be refurbished and repaired. The railroads will improve the viaducts, adding lighting and making the sidewalks wheelchair-accessible. The project will also lower Racine and Canella streets, which are located south and north (respectively) of the spot where the CTA Brown Line passes over the Metra tracks. This is being done to maintain current clearance under the UP North Line and the Brown Line.

While Metra officials took pains to emphasize that the new bridges wouldn't result in more frequent train service, they argued that the project is a net win for passengers, since it will reduce maintenance costs and maintenance-related delays.

The Union Pacific North Line runs between the Ogilvie Center in the West Loop and Evanston, Wisconsin, though most trains only go as far north as Waukegan. The rail corridor is owned by the eponymous railroad company, which operates Metra service under contract with Metra.

Metra is covering the \$243.3 project costs through a combination of state and federal funds. The commuter railroad has already received \$141.2 million, including \$6 million from 2019 Federal Transit Authority 5337 State of Good Repair Funds, and \$136.1 million from Federal Climate bond funds. Metra is still identifying the rest of the funding.

When the bridges were built, they were designed for slow trucks. The Chicago and North Western Railroad, which owned the line before Union Pacific acquired the entire company, dismantled the westbound track in the early 1980s.

Metra and UP are planning to take advantage of this. As with the Uptown Edgewater segment, they will rebuild the westbound track, then rebar what is currently the west, northbound track, and then remove the eastbound track. This way, there will always be two tracks in service at any given time. Once the project is completed, the westbound track will become the new northbound track, while the current northbound track will become a southbound track.

The net up also means that, instead of replacing an entire bridge at once, each bridge will be replaced one track at a time, so there will likely be several closures per bridge. Steve Hanks, the environmental consultant for the project, said this means construction crews would most likely be working on several bridges at once.

While the new bridges will only have room for two tracks, Hanks indicated that Metra and UP are keeping the embankment the same width to give the railroads room to triple-track the line in the future, if they choose to. Of course, that would require widening or replacing all the bridges all over again, so this may be a case of being [nice and quiet](#). After all, it's common for vehicular bridges to be built with more lanes than the roads they serve, just in case the road gets widened in the future (which is generally a bad idea for roads, whereas additional railroad tracks are often useful).

Hanks added that replacement of the bridges will require the removal of the "Welcome to Racine Village" gateway sign painted on the Racine Street bridge. "We're already discussing alternative options for different community identifiers in the area, and that's something we want to continue to work on with the Lakeview Racine Village Chamber of Commerce) and the Racine Village Neighbors (community organization)," he said.

When asked about how the project will address flooding, especially on the I-55, Hanks said that the bridges will be designed with features "to make

STREETSBLOG CHICAGO

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By Iqbal Siddiqui on Sep 13, 2021

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However, the residents didn't get many answers during Metra's September 9 virtual public meeting. The first of several planning meetings about the project. As Metra officials and consultants emphasized throughout the meeting, the details will be worked out once the railroad completes the federal environmental review and the engineering study. The design isn't expected to be completed until the summer of 2023, with construction taking place between 2023 and 2027. But the officials are expected to share more details at the second public meeting, which will take place either toward the end of 2021 or beginning of 2022.

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The Union Pacific North Line runs between the Ogilvie Center in the West Loop and Evanston, Wisconsin, though most trains only go as far north as Waukegan. The rail corridor is owned by the eponymous railroad company, which operates Metra service under contract with Metra.

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Here's some more intel about how the replacement of 11 Metra bridges in Lakeview and Lincoln Park will work, from the recent community meeting. chi.streetsblog.org/2021/09/13/mor...

Metra and 3 others

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Here's a must-read for fans of spans: [@Istudenkov](#) provides more details on the upcoming reconstruction of 11 @Metra bridges on the @UnionPacific North line.

More deets about the UPN Line bridge replacement project from the recent meeting. Nearby residents and businesses are wondering about the construction impacts, including possible flooding issues, and just how close to their homes the new ... [@UPNPlanningHQ](#)

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Here's some more intel about how the replacement of 11 Metra bridges in Lakeview and Lincoln Park will work, from the recent community meeting.

More deets about the UPN Line bridge replacement project from the recent meeting

CHI:1182738425:0582

1 Comment

Appendix C: Public Meeting #2 Summary and Comments

The following includes a brief summary of Public Meeting #2 and a table that lists the recorded comments/questions received.

The meeting was formatted as an open house with six stations consisting of a looping Project introduction video, Project overview, construction, NEPA, community feedback, and sensitive conversations. The format was intended to allow for community members to voice their specific concerns and get individual attention from experts on their respective portions of the Project. The first hour of the meeting was busy with community members circulating between the different stations and speaking with staff members.

About halfway through, a small group of residents requested a single forum where questions could be asked in front of the whole group. Staff agreed and set up a forum which lasted for the final hour of the meeting. Nearly all the questions in the forum were asked from community members who lived adjacent to the right of way and had concerns about Project impacts.

The following table lists all questions and comments that were received through Public Meeting #2 via registration, on comment cards, during individual conversations with Project team members, during the group forum, and from emails submitted to the project email address.

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Comment Card	Really need a sound wall – at least 50% of it height of rail cars to blunt noise from wheels/tracks. Do not support moving tracks west. Workers vehicles should not be permitted nearby. They should arrive on busses / vans and depart that way. 5 years of construction congestion is not fair.
Comment Card	Show us the alternatives to the blocks and blocks of retaining wall. Why is it necessary? Looks like a prison wall. Have another meeting w/ us.
Comment Card	The bridges at Wellington & Barry are regularly hit by trucks. The roads need to be lowered. Signage is not effective. Host an actual town hall meeting where the community can ask questions and receive responses not be shuffled around and ignored.
Comment Card	Support of the project. Live @ SE corner of Barry & Ravenswood. Please consider cleaning up east side of tracks of tracks b/n Barry & Wellington. Invasive trees could be removed & a green space / community garden could be installed. I pick up trash along RR side of Ravenswood on a regular basis. With the area being improved, the area would see “less dumping”. Happy to discuss further.
Comment Card	The way UP tore down trees leaves a sour taste. They really should clean up the debris. Also will you be replanting trees? I’ll be writing to all the elected officials to whom UP donates to ensure they abstain from accepting your funds so that they demand UP clean up after themselves. How would you like us to come to your property & dump all of the debris?

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Comment Card	I'm an adjacent property owner. In speaking with a representative of Metra today, she confirmed that the wall in our backyard will not be moved west toward our home. Can you please confirm for me in wording to ease my mind? Thank you, Brad. I would like to request a sound barrier along the wall in our backyard so that my 2 year old daughter is not disturbed in her sleep & generally for our quality of life. Can this be promised?
Comment Card	This project is totally ignoring the needs of the tax paying residents who live along these tracks. Who [illegible] are the "consultants" who've analyzed the impacts of this project? I'm sad, disappointed, [illegible], but unfortunately not surprised.
Comment Card	The train is already extremely close to our home [illegible]. It visibly shakes when trains go by. If the train moves 13-20' closer we have serious safety concerns for us and families. How will we be assured that safety & sound concerns will be considered or addressed? Our preference is to avoid this situation in general and only replace what is necessary which from what we can tell is the bridge. What other alternatives are being considered to avoid the [illegible] raised? What public studies have been done to show noise & safety for residents on the west side have been considered and taken [illegible].
Comment Card	(1) What permits are required for the planned work? (2) Was there any true efforts for a solution which respected the unique community this project will destroy? (3) This project feels like a cookie cutter plan put together with little to no effort for a forward looking plan to preserve our green space. (4) We know better - we should do better!! (5) The decibel level claims are not credible - I can tell the difference btwn the inbound & outbound.
Comment Card	I suggest that future community meetings be held in an auditorium setting where everyone can experience the presentation at the same time, and there will be a public Q&A session where neighbors affected by this can ask questions and receive answers in the presence of all.
Comment Card	We object to the current design moving 20-25 feet to the west. In impacts all 150 homes along the tracks and [illegible] not environmentally focused in your assessment.
Comment Card	Why wasn't there a real presentation to the community? Having standing people among two rooms, questions could not be directed, no even a microphone to share. This was not a meeting set up that allowed or encouraged communication. This was a waste of time and worse an insult. After ONE HOUR - Finally came as a group to 2nd room - that helped. Should be another such really meeting - one room - questioning.
Comment Card	We want the tracks at the public way location (Roscoe to Belmont) to stay where they are - i.e. do not shift them westward through that area, in order to preserve the lovely aesthetic that area provides to all residents. This could be done if you allowed a disruption of service during construction of that portion. While this would impact ridership, the impact is temporary. The wall being built in order to avoid this disruption

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
	is permanent, lasting the rest of our lives. Do not sacrifice this neighborhood's charm and aesthetic for the short-term benefit of disruption-avoidance.
Comment Card	The UP North Rebuild: Fullerton to Addison Project would receive federal funding and the environmental impacts of the environmental impacts air quality, project work, replace 11 bridges adjacent tracks and retaining walls, shift tracks west within existing, union pacific property, chain-link, aluminum, ashlar, wood plank, your comments will shared.
Comment Card	This is not an open public meeting. It's a confusing [illegible] where it's our job to find reps? And the police?? Really??!
Comment Card	Do what needs to be done to make this project go faster. Ignore the NIMBYs.
Comment Card	We need a public meeting with an open Q&A and a presentation to all, other than what this was. Too segmented.
Comment Card	Concern over sound, safety (water/diesel) and length of project. Also lack of visibility into project, ow it affects neighbors, etc.
Comment Card	Replacing both tracks at once puts Metra's priorities over the communities. Reducing service for a short term, make more sense than permanently impacting the community.
Comment Card	Water dispersion is a major concern, with elimination of vegetation. Visibility to proposal before finalization is critical.
Comment Card	This evening's meeting very chaotic and disorganized. Should be held in an auditorium with panel of speakers followed by Q&A. Then you can invite public to go view diagrams at various stations/tables manned by Metra people to provide additional info or clarification and have brochures, pamphlets available & your comment cards.
Comment Card	We don't want a prison wall.
Comment Card	I would like to have a noise mitigation plan. I am surprised that this was not considered a priority for Metra, as it is certainly a priority of the community.
Comment Card	(1) Storm sewer plan (2) drainage holes in retaining wall impact current systems on our property (3) concern water damage *(4) WANT copy of drainage plans (5) Drainage cannot drain into our current system so drainage holes not option for our section
Comment Card	Commitment File: (1) Request commitment file contact numbers for calls & concerns (2) Accommodating for access to garages - in/out driveway (3) Traffic flow in street when Belmont closed. Speeding, no speed bumps, safety concerns - pets/children school (4) Where store construction materials.
Comment Card	Retaining Wall (1) construction material (2) If using diesel pile hammer - diesel oil is airborne and will get in our siding & damage property (3) use latest technology for sound & vibration (4) how effect foundation of adjacent property owners (5) using h-pile, shelf pile? (6) Want renderings of retaining wall!

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Comment Card	Will you be adding sidewalk or walkway access between Diversey and Chi Che Wang park? Can trees be replanted on other city property rather than destroyed?
Comment Card	Damage Concerns! (1) Brick on our private driveway (2) dust/debris on home (3) Hardship/inconvenience/impact daily lives (4) complete one area of construction before moving on to next area / length time per area
Comment Card	I find it hard to believe you can't find a way to improve noise pollution.
Comment Card	Everyone agrees that the bridges need to be replaced. However, with ridership down, why has the scope of this project expanded so significantly. It is going to have a large negative impact on many lives & no one is asking for this change.
Comment Card	Thanks for opening up to feedback! Please consider installing concrete sleepers and noise barriers to reduce noise pollution in the neighborhood.
Comment Card	This would not be happening in Winnetka. Don't take advantage of us in Chicago.
Comment Card	Please explain the sound methodology - the posters and info give an impression of the impact that is absolutely misleading.
Comment Card	Someone needs to clean up those bulldozed trees ASAP.
Comment Card	We need some trees from Roscoe to Belmont to block some of the noise. Also, I like the planters for flowers.
Comment Card	Issues I'm most concerned with: (1) Bike lane access on Roscoe & School during construction (2) Repainting Roscoe Village Sign (3) Maintain green space (tree canopy & flowers) Thank you! - Walter
Comment Card	Good luck. I hope things go well
Group Forum	Guaranteed to not have flood damage for how long? Has there been a water diversion study?
Group Forum	Concerns on diesel impacts, smell
Group Forum	What is the obligation for full funding for the project?
Group Forum	Who has decision making authority for impacts on the environment? Will the project go for CE?
Group Forum	Can we submit an appeal to FTA directly?
Group Forum	When do you anticipate final submission to FTA?
Group Forum	Can community get access/info on the EIS? Can community get contact info for FTA rep?
Group Forum	Why do the tracks have to shift? Why can't they roll in bridges? (cite examples on southside claiming it took a weekend)
Group Forum	Why can't you do the 'track shift east' option?
Group Forum	Will there be three tracks?
Group Forum	What are our rights as adjacent property owners? (easement, safety, environment) Can and easement be refused? What if there is city property between private property and UP?
Group Forum	What is the plan to financially compensate owners?
Group Forum	Where and when was the noise study?
Group Forum	When will noise/vibration study be available? Will it be final?

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Group Forum	Concerns about handling landscape maintenance at Ravenswood/Cornelia; taking too long to clear dead trees, community not informed
Group Forum	Can we submit an appeal to FTA directly?
Group Forum	Can community get access/info on the EIS? Can community get contact info for FTA rep?
Group Forum	Concerns on disrupting service vs. disruption residents' quality of life in long term
Group Forum	Can Metra commit to another public meeting?
Registration	Why do you think it is ok to wreck the gardens and animal habitats and put a wall up instead? It is unacceptable.
Registration	Is there an opportunity for residents to discuss with engineers to make suggestions for design to optimize the project for both Metra and residents?
Registration	Have there been any considerations to use this budget to electrify the line and replace the current diesel rolling stock?
Registration	Will this project include any measures that enable electrification of the line at some point in the future?
Registration	Is the estimated unit cost per bridge competitive with similar projects across America, and our OECD cohort countries with commuter rail?
Registration	What legal entities are parties to this project? What legal requirements are necessary for this project to be approved? What governing bodies have jurisdiction This is my formal request for all information necessary to understand what elements of this project are material to my rights as an adjacent landowner.
Registration	When will construction starts and where will it start? When will construction take place and will there be any work during the weekend?
Registration	Timing of track repairs on Ravenswood between school and Roscoe
Registration	How far west are the tracks moving?
Registration	Why do the tracks need to move so far west so they are on top of our front door? There has to be another option!
Registration	Why are you putting up a wall that looks like a jail wall
Registration	Are smaller bridges being prefabricated to reduce on-site installation time?
Registration	Why doesn't Metra use drop in bridges built offsite?
Registration	I did not register in time for the 30 min consult. I have property that backs up to the tracks, and want to learn more about the noise barrier plans, similar to properties north of Montrose.
Registration	How will it affect adjacent homes generally. And specifically
Registration	I have several questions. First, I want to know the proposed scope of work along the block of 3000-3099 N Honore. Second, what is the proposed

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
	timeline. Three, describe how you would be carrying out this plan and how we can intervene if the workers deviate from the plan
Registration	Are the bridges going to allow for the construction of protected bike lanes in the underpass? Will there be any support columns that would potentially block protected bike lanes from being constructed in the future?
Registration	Water track drainage, sound/vibration issue with retaining wall
Registration	Is noise pollution of the railway being considered in the scope of this project? Is there budget to install noise abatement measures like noise screens, long-welded rails, concrete sleepers, and composite brake shoes?
Registration	Why did you all decide to just knock down a bunch of trees along the tracks and leave them there for months? Relatedly, when are you going to clean it up?
Registration	Why have we not explored alternatives to destroying the surrounding landscape/buffer?
Registration	Would like to keep all trees or replant new trees. Would like to know if there will be a wall built along Ravenswood Street or same as it is now or will there be a wall built?
Registration	What will happen with the iconic roscoe village art.
Registration	What contracts and agreements are available to the public for review related to the various entities tied to this project? (Metra, state, county, municipal, Union Pacific) Which entities are material stakeholders in this project? What easements are required for project approval? What are my rights as an adjacent land owner? What information is obtainable via the freedom of information act? What information and topics are not required to be disclosed to the public? Does any element of this project require eminent domain?
Adj Resident Liaison Emails	Request an individual meeting to discuss the project. Previously emailed Melody in February
Adj Resident Liaison Emails	Requested 2nd onsite Picardy Place meeting at Open House. Emailed Melody with request for graphic of track shift
Adj Resident Liaison Emails	Requested immediate phone call to discuss the project. Has a pending sale at Picardy Place
Adj Resident Liaison Emails	Wants to know how this affects their home (eastside resident, north of Belmont) regarding noise, views, trees, duration of project.
Adj Resident Liaison Emails	Had list of action items to address concerns presented at open house. More details on alternative options, additional public forum, draft documents of environmental review
Adj Resident Liaison Emails	Owns two properties west of tracks. Wants to confirm that retaining wall would not be on property and determine how to preserve two trees. Also concerned about drainage and construction length
Adj Resident Liaison Emails	Suggested onsite group meeting with Picardy Place.

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Adj Resident Liaison Emails	Information on effects to property/outside area.
Adj Resident Liaison Emails	More detailed design and construction information.
Adj Resident Liaison Emails	Request for redesign/other alternatives and noise mitigation
Adj Resident Liaison Emails	Comments following Picardy meeting. Mostly about retaining walls and request for noise mitigation.
Emails	Will the refurbishment of the Addison/Lincoln Bridge include devices to prevent birds from living under the bridges and discharging waste on the sidewalks?
Emails	I am a rider and a resident who strongly supports the updates being made between Fullerton and Cornelia. Infrastructure upgrades are valuable to me and others who rely on the service.
Emails	I'm an architect with an office nearby and creator of the garden on the west side of Ravenwood. Twenty years ago, received a grant from Green-corps to design and landscape the community garden. Can I have a private meeting to share my concerns about the upcoming work and design?
Emails	As a resident and frequent commuter, I hope that the timeline and budget proposed for the project remains the same. Agree the bridges should be replaced and acknowledges that Metra plans to minimize interruptions. No concerns at this time regarding the linked Blockclub Chicago article.
Emails	The community was able to share feedback on walls, fences, and vegetation but I'm more concerned about the track shift and finding an alternative to this. Please have another forum and go back to sharing redesign options that don't include the tracks being moved closer to our homes. Metra service will be impacted temporarily, the community will be impacted long term.
Emails	I attended the Burley meeting and agreed that all bridges need to be rebuilt. I recommend replacing all the trees too. The new west wall looks great and should keep people off track. Moving the track west and going down to two tracks makes sense.
Emails	I'm a Zone A to Zone J commuter on the UP-North Line. Remain steadfast in your commitment to the rides. Getting to work on time is important to passengers like me.
Emails	Could I be added to the email list for future public project updates?

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Emails	Thanks for hosting the open house. I'm embarrassed that my fellow residents. Our neighborhood needs modern, high-quality infrastructure that improves the community, and this project will absolutely do that. Please forge ahead with the most time and cost-efficient design.
Emails	This is a good project, go full steam ahead and don't listen to the NIMBY's who are trying to stop it.
Emails	The new wall and tracks will lock us in our yards. I hope there's not an emergency where we need to get out from the back of our homes. Ridership is down with COVID so why is a third line needed?
Emails	I appreciated the courtesy and attentiveness of the Metra, Benesch, and CDM reps. at the open house. I hope that Roscoe Street and Cornelia Avenue are not closed simultaneously due to lowering the roadbeds of these two streets as they tend to safely relieve traffic during Cubs games, street festivals and other events.
Emails	I'm upset that block of tress down Ravenswood will be eliminated and replaced by an ugly retaining wall. In order to avoid disrupting Metra service, homeowners will have their gardens ruined so they can protect the customer experience. Riders can be inconvenienced. What about the local wildlife?
Emails	Please reconsider the design of this project. This seems like a safety hazard if there were ever a derailment. We realize the bridge on Diversey needs to be upgraded but our biggest concern is the significant move of the tracks west.
Emails	I live in Picardy Place neighborhood and will be directly affected. The train will be feet from my property line. What's the reason why a sound barrier above the retaining wall hasn't been considered? What's the decision-making process for this plan?
Emails	I'm a first-time home buyer in Picardy Place. I'm concerned about flooding and the foundation cracking, the sound of the trains, and question if there have been any noise studies or analysis done.
Emails	This is my third home near a train. I enjoy the train where it currently is. I do not want the track even closer to my home. You say on the project website that the Single-Track and Shoofly options you considered were rejected because they would result in service delays that exceed acceptable levels which is temporary but the impact on the residents will be permanent.
Emails	Attached is a document that details the feedback from the residents of 1801 W Diversey Pky, Wolcott Row Association. This project directly

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
	impacts us. We ask for open communication and transparency throughout this process.
Emails	A FOIA request for the draft environmental review was made on behalf of the Adjacent Neighbors in early April 2022. In response, Metra filed for an extension. How can we properly review the document when you refuse to provide it
Emails	Do not build the tracks closer to our backyard. Please reconsider. The noise is loud, and our house shakes when the trains ride by. My major concern is excessive water runoff in my basement.
Emails	The major concern is the noise that is going to impact their work and sleep and losing their backyard. Another major concern is their basement which can be flooded by the runoff.
Emails	Not shifting the tracks west closer to our properties is preferred, for it would drastically affect my family and many others' way of life by increasing noise pollution, fumes, as well as vibration. If switching the tracks west is done, then the project must include a noise insulation/mitigation program to reduce the negative aspects of the Metra UP North Line project. All affected families must-have new three-layered glass windows and new insulation installed in their homes to mitigate the impact at no cost to them.
Emails	The main concerns are noise, vibrations, and overall impact on its proprietary.
Emails	The resident is requesting Metra to help her family mitigate the impacts of the construction by providing support: changing their windows and doors to acoustically rated windows, and rear doors, and reinforcing wall insulation.
Emails	The project will impact their backyard and their proprietary borders.
Emails	Vegetation that is damaged and removed needs to be restored. Prioritize the dwindling Tree Canopy alongside a Variety of Pollinator Plants. What areas were studied for the Noise and Vibrations Impact? What measures are taken to combat the diesel /pollution problem and water drainage?
Emails	Receive project updates.
Emails	The main concern is pollution and how this project will have an impact on the health of the residents (increased risk of lung cancer). Another concern is the value of homes that will decrease because of this project.
Emails	The main concern is that there are no sufficient data/studies: for water displacement, noise, and vibration studies (the study should also have data for the winter period), and concrete examples of impacts that will be done on adjacent homeowners.
Emails	The noise and vibration study are not accurate. The study should show the impact of a sound in a specific moment, not on a 24-hour average.
Emails	Where were the noise and vibration studies conducted? What are the plans for landscaping replacements? When will Wolcott row / the Diversey bridge be worked on?

Public Meeting #2 Questions and Comments	
Question Source	Question/Comments
Emails	Moving the tracks close to the houses will have a lot of risks: safety (trains can derail); noise and pollution impacts; security risks and privacy loss because the fence will be too close to the balconies and backyards, etc.
Emails	The main concern is the safety of the kids living so close to the train, also having the fence so close will allow trespassers to invade private property.
Emails	Metra does not provide a clear reason why shifts the wall so far west/close to their homes. Main concerns are regarding vegetation, noise and pollution. Environmental study information study is not available to the public. Metra to be more transparent about regulations that they are accountable for meeting in order to safely operate in residential areas.
Emails	Metra to investigate an alternative that will not require tree removal alongside the west side of the tracks. The debris of the constructions can be a hazard for the kids playing in the backyard. "Importantly, I was told during the open house that the retaining wall in my back yard will not shift west. The new wall will lie in the same place that it is today. Could you please commit to this in writing for me?"
Emails	Metra to investigate an alternative that will not require tree removal alongside the west side of the tracks. The debris of the constructions can be a hazard for the kids playing in the backyard. "Importantly, I was told during the open house that the retaining wall in my back yard will not shift west. The new wall will lie in the same place that it is today. Could you please commit to this in writing for me?" Redi-Rock is a preferred option for pre-cast concrete walls.
Emails	The noise and vibration study should have more information. Where was this study conducted (outdoor, the extent of it), and where can the residents get full access to it? The Union Pacific/Metra/City of Chicago should consider acquiring Picardy Place Subdivision, as this seems to be prime property to consider for an additional service stop, with gated parking, increase in revenue, ridership, and decrease in overall carbon footprint.
Emails	Main concern is the vegetation removal.
Emails	Quality of life will be impacted due to noise and pollution and the value of homes will decrease. Metra should investigate providing some sort of funds to the community affected by this project.

Appendix D: Public Meeting #2 Notifications and Media Coverage

The following documents show how and where Public Meeting #2 notifications were made to inform the community of the meeting as well as media coverage that was garnered leading up to and after the public meeting.

Public Meeting Notifications

1) Email notifications – Began on March 25, 2022



Hello neighbors,

Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on April 27 from 6 p.m. to 8 p.m. at Burley Elementary School, 1630 W Barry Avenue.

Open house details and a registration link will be provided at a later date. Registration is encouraged but not required.

Stay in touch!

Learn more at [Metra.com/UPNrebuild](https://metra.com/UPNrebuild).

For Project comments and questions, email us at UPNrebuild@metrarr.com.

For adjacent property concerns, email Melody Carvajal at carvajalma@edmsmith.com.

For media inquiries, please contact Michael Gillis at mgillis@metrarr.com.



April 27, 2022 | 6:00 PM - 8:00 PM
Burley Elementary School | 1630 W Barry Ave

Join us to learn and ask questions about the:

- Need and process to modernize our 120-year-old bridges and track structures
- Environmental Review and Analysis Results
- Proposed shifts in track locations
- Timeline and next steps

[Click here to register, and stop by anytime!](#)

Registration is encouraged, but not required.

ADA Accommodations

The location is ADA accessible and American Sign language interpreter services will be provided at the open house. To request additional materials for the hearing impaired, or if you need materials for the visually impaired, please email upnrebuild@metra.com.

Language Accommodations

Spanish language interpreter services will be provided at the open house. If you need additional foreign language assistance, please email upnrebuild@metra.com.

Stay in touch!

Learn more at metra.com/UPNrebuild.

For Project comments and questions, email us at UPNrebuild@metrarr.com.

For adjacent property concerns, email Melody Carvajal at carvajalma@edmsmith.com.

For media inquiries, please contact Michael Gillis at mgillis@metrarr.com.



Good afternoon neighbors,

Last week, Metra hosted an open house for the UP North Rebuild: Fullerton to Addison Project. The open house format was used to accommodate differing schedules and provided an opportunity for attendees to learn about components of the Project that they were most interested in. Additionally, to be responsive to attendees' requests for a more global question and answer format, Project staff answered questions as a panel.

The materials presented at the open house have been posted to the Project's website to ensure that the community has the most up-to-date information, and we've linked them below for your reference.

- [Video Presentation](#)
- [Video Presentación con subtítulos en español](#)
- [Brochure](#)
- [Construction Options Considered Overview](#)
- [Project Overview Exhibit Boards](#)
- [Construction Overview Exhibit Boards](#)
- [NEPA Overview Exhibit Boards](#)
- [Community Voices Exhibit Boards](#)

At the open house, the community was encouraged to provide comments that would be shared with the Project design team, the city, and the alderman's office to inform the design efforts for retaining walls and fencing and potential options to restore vegetation in the public way. To submit your comments, please submit via the Project email address: UPNrebuild@metrarr.com.

Comments are encouraged to be submitted by May 23 to be incorporated into the environmental review process and for design considerations.

The [Frequently Asked Questions](#) page will continue to be updated in order to address questions and comments that are received.

Stay in touch!

Learn more at metra.com/UPNrebuild.

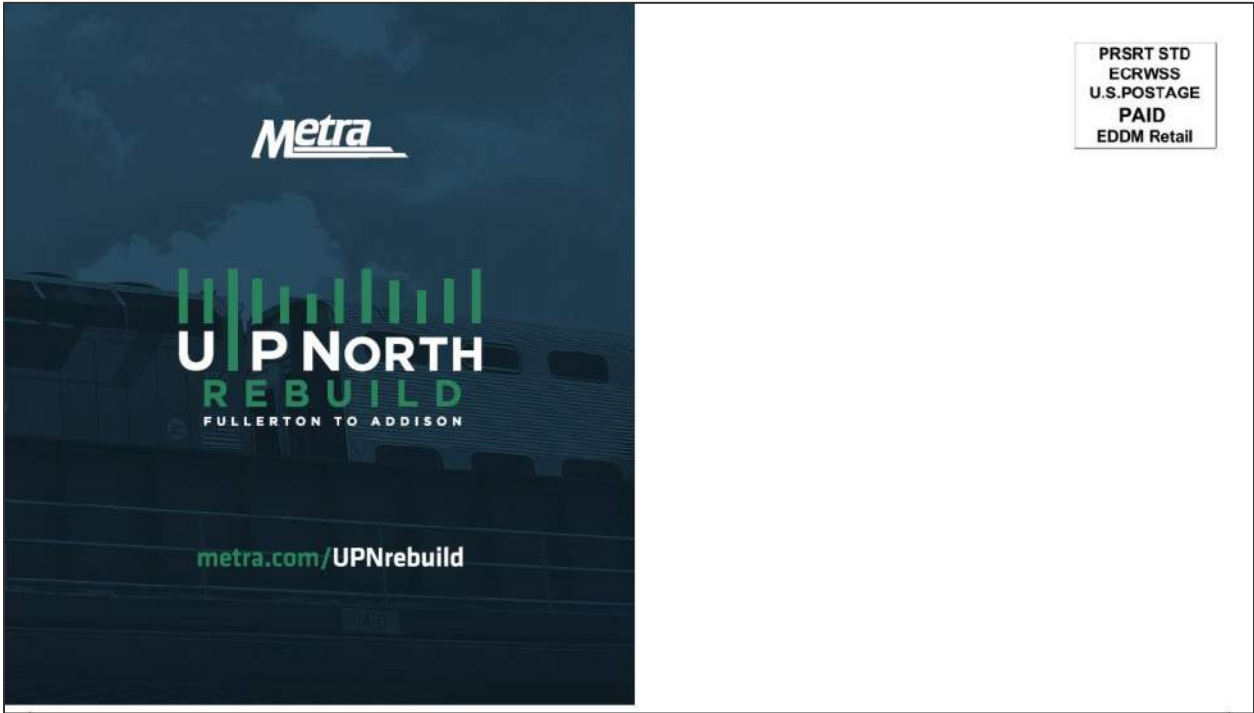
For Project comments and questions, email us at UPNrebuild@metrarr.com.

For adjacent property concerns, email Melody Carvajal at carvajalma@edmsmith.com.

For media inquiries, please contact Michael Gillis at mgillis@metrarr.com.



2) Postcard Mailers - Delivery began on April 6, 2022




3) Flyers at the Ravenswood and Clybourn Metra stations - Posted on April 19, 2022



6) Social media notifications and shares

M Metra
1d · 🌐

Metra will hold a public open house on April 27 to inform adjacent property owners and the public about its progress on the UP North Rebuild: Fullerton to Addison Project, which will replace aging bridges over 11 streets between Fullerton and Addison along the UP North Line in Chicago. Read more: <https://bit.ly/3DKgRPM>



👍👍 9 2 Shares

🔄 Metra UP-N Retweeted

M Metra · @Metra · Apr 6

Metra will hold a public open house on April 27 to inform adjacent property owners and the public about its progress on the UP North Rebuild: Fullerton to Addison Project, which will replace aging bridges over 11 streets between Fullerton and Addison: bit.ly/3DKgRPM



👍 🔄 3 ❤️ 4 📎

M Metra · @Metra

Metra will hold a public open house on April 27 to inform adjacent property owners and the public about its progress on the UP North Rebuild: Fullerton to Addison Project, which will replace aging bridges over 11 streets between Fullerton and Addison: bit.ly/3DKgRPM




12:00 PM · Apr 6, 2022 · Sprout Social

3 Retweets 4 Likes

West Lakeview Neighbors
March 25 at 6:22 PM · 🌐

<https://mailchi.mp/853.../thank-you-for-attending-8869398...>



MAILCHIMP

Save the Date | UP North Rebuild Open House
Hello neighbors, Metra will be holding a public open house for the UP North Rebuild: Fullerto...

👍 2 1 Share

Gross Park Neighbors Foundation
 March 27 at 4:13 PM · 🌐

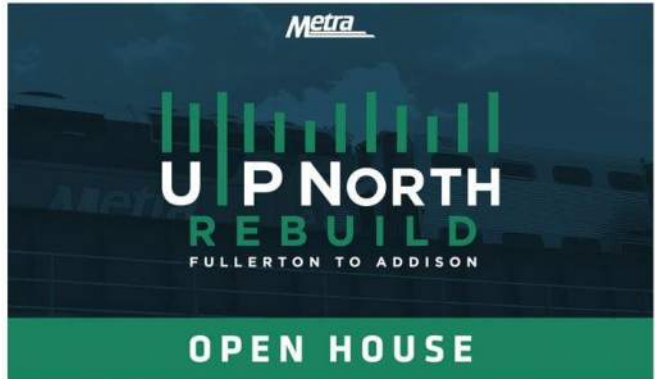
Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on April 27 from 6 p.m. to 8 p.m. at Burley Elementary School, 1630 W Barry Avenue. Open house details and a registration link will be provided at a later date. Registration is encouraged but not required.



Like Comment Share 1

Sara Feigenholtz, State Senator is at Burley Elementary School.
 March 30 at 7:30 PM · Chicago · 🌐

Our Office has received a number of calls from concerned constituents regarding Metra's UP North Rebuild: Fullerton to Addison Project. Metra will be holding a public open house to share project updates from 6pm to 8pm on April 27 at Burley Elementary School, 1630 W Barry Avenue. Open house details and a registration link will be provided at a later date. Registration is encouraged but not required. For more information, please visit <https://metra.com/UPNrebuild>



This is a public open house for Metra's UP North Rebuild: Fullerton to Addison Project.

Join Metra on Wed, April 27 from 6-8pm at Burley Elementary School to learn about project updates and ask questions about project updates, including this:

- Need and process to modernize our 120-year-old bridges and track structures
- Environmental review and analysis summary
- Proposed shifts in track locations
- Timeline and next steps

[REGISTER HERE](#)

Newsletter · Community Blog

West Lakeview Neighbors
 3d · 🌐

Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on April 27 from 6 p.m. to 8 p.m. at Burley Elementary School, 1630 W Barry Avenue. Open house details: [Metra.com/UPNrebuild](https://metra.com/UPNrebuild).

Metra is asking residents immediately adjacent to the railroad to attend the meeting or reach out directly to Metra with concerns. The community is welcome to provide comments and ask questions via email at UPNrebuild@metra.com. To sign up for an individual or group discussion, contact the Adjacent Property Owner Liaison, Melody Carvajal, at carvajalma@cdmsmith.com.

Metra Construction Overview
 The Metra UP North Rebuild: Fullerton to Addison Project along Metra's Union Pacific North (UP-N) Line consists of the following project elements:

- The replacement of 11 railroad bridges from Fullerton to Cornelia
- The replacement of retaining walls where required from Fullerton to Addison
- Shifting tracks west between Fullerton and Addison to align with the existing tracks north and south of the project area
- The lowering of Roscoe and Cornelia to maintain current clearance under the roadway and the CTA Brown Line
- Refurbish, including painting, the existing Lincoln/Addison bridge
- Some utility work along the project corridor

Further project details and updates are provided in the Project's Frequently Asked Questions below:
<https://metra.com/UPNrebuildFAQs>





Ann M. Williams - State Representative

April 26 at 12:52 PM

The Metra Up North Rebuild Open House is tomorrow night at Burley Elementary from 6-8PM! Join neighbors and representatives from Metra to discuss the upcoming modernization project.

Registration is encouraged but not required: <https://www.eventbrite.com/.../metra-up-north-rebuild...>



April 27, 2022 | 6:00 PM - 8:00 PM
Burley Elementary School | 1630 W Barry Ave

Join us to learn and ask questions about the:

- Need and process to modernize our 120-year-old bridges and track structures
- Environmental Review and Analysis Results
- Proposed shifts in track locations
- Timeline and next steps

1



Rep. Ann Williams

@RepAnnWilliams

The Metra Up North Rebuild Open House is tomorrow night at Burley Elementary from 6-8PM! Join neighbors and representatives from Metra to discuss the upcoming modernization project.

Registration is encouraged but not required: [eventbrite.com/e/metra-up-nor...](https://www.eventbrite.com/e/metra-up-nor...)



April 27, 2022 | 6:00 PM - 8:00 PM
Burley Elementary School | 1630 W Barry Ave

Join us to learn and ask questions about the:

- Need and process to modernize our 120-year-old bridges and track structures
- Environmental Review and Analysis Results
- Proposed shifts in track locations
- Timeline and next steps

12:54 PM - Apr 26, 2022 - Twitter Web App



Lakeview Roscoe Village Chamber of Commerce

April 16 at 10:30 AM

Check out the Metra UP North Rebuild: Fullerton to Addison Project Open House on Wed. April 27 from 6-8pm at Burley Elementary School to learn and ask questions about the need and process to modernize our 120-year-old bridges and track structures, Environmental Review and Analysis Results, proposed shifts in track locations, and timeline and next steps.



EVENTBRITE.COM

Metra UP North Rebuild: Fullerton to Addison Project Open House

This is a public open house for Metra's UP North Rebuild: Fullerton to Addison Project.

2

1 Share



Roscoe Village Neighbors

April 16 at 9:40 AM



Lakeview Roscoe Village Chamber of Commerce

April 16 at 10:30 AM

Check out the Metra UP North Rebuild: Fullerton to Addison Project Open House on Wed. April 27 from 6-8pm at Burley Elementary School to learn and ask questions about the need and process to modernize our 120-year-old bridges and track structures, Environmental Review and Analysis Results, proposed shifts in track locations, and timeline and next steps.



EVENTBRITE.COM

Metra UP North Rebuild: Fullerton to Addison Project Open House

This is a public open house for Metra's UP North Rebuild: Fullerton to Addison Project.



7) Aldermanic e-Newsletters



32nd Ward Newsletter: April 1, 2022

Dear Neighbor

Street sweeping has started for the 2022 season. Please review the map below for dates when your block will be swept. Street sweeping helps keep the curbs clear of garbage and debris that can jam up the sewers and also prevent excess debris from entering the city drainage system.

The UP North Metra line replacement project is underway in the 32nd Ward. On April 27th, you can meet with reps from the project to discuss individual concerns as they work to replace the 11 bridges and rebuild retaining walls in the project area of Fullerton to Addison Avenues. This is a long term project already underway north of the 32nd Ward. The work has to be done. It will require train line rail realignment, and repairs that will cause disruption on properties owned by UP and along the streets that the bridges cross. Please read below for details and links to the project.

The Chicago Board of Elections is looking for judges for the June and November 2022 and February 2023 elections. See below for details. Young adults and teens interested in elections are encouraged to apply, and can also work through the [Mikva Challenge](#) program.

The Police Department and other city agencies are hosting three more violence prevention and public safety events. See below for the dates and how to participate.

I've added a note on the ward map redistricting, with a couple of draft map pics. There are still ongoing discussions about the maps but the drafts give a sense of the potential 32nd Ward and surrounding boundaries.

Also included in the newsletter: information on several job openings, Low Income Drivers Debt Payment Relief plans, Easter events for the family, Kidical Mass is back, casino proposal public meetings, and a way to help Ukraine.

Have a nice weekend,

Scott Waguespack
Alderman



Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on **April 27 from 6 p.m. to 8 p.m.** at **Burley Elementary School, 1630 W Barry Avenue.**

Open house details: [Metra.com/UPNrebuild](https://metra.com/UPNrebuild).

Metra is asking residents immediately adjacent to the railroad to attend the meeting or reach out directly to Metra with concerns. The community is welcome to provide comments and ask questions via email at UPNrebuild@metrarr.com.

Each property is unique; therefore, we are offering one-on-one sessions to meet with Project staff. To sign up for a session, contact the Adjacent Property Owner Liaison, Melody Carvajal, at carvajalma@cdmsmith.com.

Metra Construction Overview

The Metra UP North Rebuild: Fullerton to Addison Project along Metra's Union Pacific North (UP-N) Line consists of the following project elements:

- The **replacement of 11 railroad bridges** from Fullerton to Cornelia
- The **replacement of retaining walls** where required from Fullerton to Addison.
- **Shifting tracks** west between Fullerton and Addison to align with the existing tracks north and south of the project area
- The **lowering of Roscoe and Cornelia** to maintain current clearance under the roadway and the CTA Brown Line
- **Refurbish**, including painting, the existing Lincoln/Addison bridge
- **Some utility work along the project corridor**

Further project details and updates are provided in the Project's Frequently Asked Questions section [here](#).



32nd Ward Newsletter: April 8, 2022

Dear Neighbor

Neighborhood Clean & Green Events- Earth Day is right around the corner. If you are organizing a clean and green event in your neighborhood, please email details to info@ward32.org and we'll happily promote it! The Chicago Park District and Friends of the Parks online volunteer registration is open for their annual Earth Day Celebration and Clean-up. Check out the list of cleanups at parks and preserves on www.earthdaychicago.org. 85 parks have registered already with site captains who will organize the clean and green. Other neighbors are organizing alley or block cleanups, so check in with your neighbors to do a block building event in your neighborhood.

High school students interested in learning more about civil rights can sign up for the upcoming Civil Rights Student Leadership Conference. The conference brings together high school students from across the city to hear from speakers and participate in workshops on civil rights issues. Sign up below.

The Office of the City Clerk is bringing back senior hours beginning Tuesday, May 3, 2022 and offering them every Tuesday through July 26, 2022. As a reminder, senior hours are 8-9 a.m. for individuals 65+ at the Archer and Gale satellite office locations.

There are two updates below for the California water main and Armitage water main infrastructure projects. We are checking these frequently for potholes and other needs as crews prepare for the spring repaving. Asphalt plants will be re-opening soon, so repaving will be right behind the openings.

A major reconstruction project on the **Metra UP North** is taking place through the 32nd Ward. The Metra-UP North project area will run from **Fullerton to Addison and include rebuilding of viaducts, utility, rail and easement areas.** If you live next to the rail, the upcoming meeting is important for you to attend, or you can set up a meeting with project coordinators. This is an important infrastructure project that has been years in the making with construction work starting soon. Here is some [history of the UP North](#). [Metra To Rebuild 11 Old Bridges Through Lakeview As Part Of Union Pacific North Line Overhaul](#)

Chicago Casino RFP Information- The City of Chicago held several meetings for casino operators to present their ideas for a casino to the public this week. I've added the process for selecting a casino below with links to the full set of RFP proposals by potential casino operators.

Catalytic converter fines hit again at different times of the day in the 14th District. If you hear cutting on your block that just doesn't sound right, try to identify any vehicle you see, and call 911 immediately. CPD has sent out the alert below.

The **Chicago Department of Business Affairs and Consumer Protection (BACP)**, will host a remote BACP Small Business Expo on Thursday, May 6th from 9:00 a.m.-1:00 p.m. The interactive expo is part of the City's effort to support neighborhood businesses and economic development by leveraging local business support resources. See details below.
Reminder - Monday's street sweeping will be in South Lakeview Zone 8 so be prepared to move your vehicle for the city sweeper.

Have a nice weekend,

Scott Waguespack
Alderman

Metra UP North Rebuild Open House

Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on **April 27 from 6 p.m. to 8 p.m.** at **Burley Elementary School, 1630 W Barry Avenue.**

Open house details: metra.com/UPNrebuild.



Metra is asking residents immediately adjacent to the railroad to attend the meeting or reach out directly to Metra with concerns. The community is welcome to provide comments and ask questions via email at UPNrebuild@metrarr.com.

To sign up for an individual or group discussion, contact the Adjacent Property Owner Liaison, Melody Carvajal, at carvajalma@cdmsmith.com.

Metra Construction Overview

The Metra UP North Rebuild: Fullerton to Addison Project along Metra's Union Pacific North (UP-N) Line consists of the following project elements:

- The **replacement of 11 railroad bridges** from Fullerton to Cornelia
- The **replacement of retaining walls** where required from Fullerton to Addison.
- **Shifting tracks** west between Fullerton and Addison to align with the existing tracks north and south of the project area
- The **lowering of Roscoe and Cornelia** to maintain current clearance under the roadway and the CTA Brown Line
- **Refurbish**, including painting, the existing Lincoln/Addison bridge
- **Some utility work along the project corridor**

Further project details and updates are provided in the Project's Frequently Asked Questions section [here](#).



32nd Ward Newsletter: April 15, 2022

Dear Jami,

Last week, the Mayor released a 5-year vision of trails and open spaces that will contribute to a network of community-led green infrastructure projects that promote the health and well-being of Chicago residents and visitors. This vision complements the progress backed by many neighborhood organizations over the past few years called the North Branch Park and Preserve. On the north side, the vision is to make waterfront and trail improvements and connect more communities and parks along the North Branch of the Chicago River and the North Branch Channel. Read below for an update on one piece of this connection, the [General Trust](#) and [Oliver Avenue](#). We will add more details in the next few weeks.

June 28th is statewide election day. Sign up now via the [Chicago Board of Elections \(CBE\)](#) to become an election judge and to learn how to apply to vote by mail. The number of polling locations has decreased over the years with former sites being redeveloped or unable to meet the ADA needs of the CBE. The CBE is always looking for new sites with us, but they are also making a push to vote by mail. Vote early at many other locations, and reduce the pressure on election day locations. Details on how to vote by mail are listed below.

Former Alderman Danny Solis pleaded not guilty to a bribery charge in federal court this week. In 2018, he [sponsored](#) a [district](#) [announcement](#) [process](#). The City is asking to intervene as a victim, so the judge will reconvene his hearings in five weeks. If Solis is not held accountable for the bribery charge, he walks away with little to no accountability, although he appears to have provided unprecedented evidence against some very powerful elected officials. The taxpayers are the ones that are at a loss on this whole scheme for having dishonest elected officials working on their own behalf, as opposed to for the taxpayers.

Don't forget to check your vehicle before the weekend, as street sweeping continues through DuSable and travels through to Logan Square.

Have a nice weekend,

Scott Waguespack
Alderman

**REMINDER for Residents Along the UP NORTH METRA
Metra UP North Rebuild Open House**

Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on **April 27 from 6 p.m. to 8 p.m.** at **Burley Elementary School, 1420 W Barry Avenue.**

Open house details: metra.com/UPNRebuild.

Metra is asking residents immediately adjacent to the railroad to attend the meeting or reach out directly to Metra with concerns. The community is welcome to provide comments and ask questions via email at UPNRebuild@metra.com.

To sign up for an individual or group discussion, contact the Adjacent Property Owner Liaison, Meboby Carvajal, at carvajalmeb@metranorth.com.

Metra Construction Overview

The Metra UP North Rebuild: Fullerton to Addison Project along Metra's Union Pacific North (UP-N) Line consists of the following project elements:

- The replacement of 11 railroad bridges from Fullerton to Cornelia
- The replacement of retaining walls where required from Fullerton to Addison
- Shifting tracks west between Fullerton and Addison to align with the existing tracks north and south of the project area
- The lowering of Roscoe and Cornelia to maintain current clearance under the roadway and the CTA Brown Line
- Retainable, including painting the existing Lincoln/Addison bridge
- Some utility work along the project corridor

Further project details and updates are provided in the Project's [Frequently Asked Questions](#) section [here](#).



32nd Ward Newsletter: April 22, 2022

Dear Jami,

A reminder that Metra will be holding a public open house for the UP North Rebuild on Wednesday, **April 27 from 6 p.m. to 8 p.m.** at Burley Elementary School. More details are listed below.

The Chicago Department of Housing's (DOH) **Small Accessible Repairs for Seniors (SARES) Program** provides small repairs that increase home safety, security, and accessibility for seniors who are 60+ years of age and earn no more than 80% of the area medium income. We've added some of the details below but please feel free to call or stop by if you need further assistance.

Street sweeping for 2022 is underway. Please be on the lookout for signs and make sure to move your vehicle on your scheduled day (south and east sides of the street are normally done on the first day, north and west on the second; please watch for signs). Share the schedule with your neighbors.

Have a nice weekend,

Scott Waguespack
32nd Ward Alderman

**REMINDER for Residents Along the UP NORTH METRA
Metra UP North Rebuild Open House**

Metra will be holding a public open house for the UP North Rebuild: Fullerton to Addison Project on **April 27 from 6 p.m. to 8 p.m.** at **Burley Elementary School, 1420 W Barry Avenue.**

Open house details: metra.com/UPNRebuild.

The community is welcome to provide comments and ask questions via email at UPNRebuild@metra.com. To sign up for an individual or group discussion, contact the Adjacent Property Owner Liaison, Meboby Carvajal, at carvajalmeb@metranorth.com.

Metra Construction Overview

The Metra UP North Rebuild: Fullerton to Addison Project along Metra's Union Pacific North (UP-N) Line consists of the following project elements:

- The replacement of 11 railroad bridges from Fullerton to Cornelia
- The replacement of retaining walls where required from Fullerton to Addison
- Shifting tracks west between Fullerton and Addison to align with the existing tracks north and south of the project area
- The lowering of Roscoe and Cornelia to maintain current clearance under the roadway and the CTA Brown Line
- Refurbish, including painting, the existing Lincoln/Addison bridge
- Some utility work along the project corridor

Further project details and updates are provided in the Project's [Frequently Asked Questions](#) section [here](#).



32nd Ward Newsletter: May 6, 2022

Dear Jami,

The Wainwright Neighborhood Association will be hosting their monthly community meeting with 19th District Commander Arin Jessan Monday at 6:30. Commander Jessan, along with CANS officers, will be present to answer any questions you may have and give a brief update on the state of public safety in the 19th District, including addressing the police response to a series of armed robberies and shootings in the 19th. For more information including the [registration link](#), [click here](#). This is a virtual meeting. More details about the robberies and shooting are listed below in the Police Alert.

City crews have removed the temporary existing asphalt on Armitage Avenue between Damen and Vilson Avenues and will be prepping the base and repaving it over the next few days. Repaving should take about a week to complete. Paving on Armitage is allowed during non-work hours for the crews but cars should be removed early each morning.

Bally's has been chosen as the operator of the state licensed casino. There is a Town Hall scheduled for next Thursday, May 12 from 7:00pm to 9:00pm at the UIC (radore and Sade, Datin Forum, 723 W Roosevelt Road). Representatives from the city and Bally's Corporation will be there to share additional information, respond to questions, and hear from community members.

Have a nice weekend,

Scott Waguespack
32nd Ward Alderman

UP North - Metra Project Update

Last week, Metra hosted an open house for the UP North Rebuild: Fullerton to Addison Project. The open house format was used to accommodate differing schedules and provided an opportunity for attendees to learn about components of the Project that they were most interested in. Additionally, to be responsive to attendees' requests for a more global question and answer format, Project staff answered questions as a panel.



The materials presented at the open house have been posted to the Project's website to ensure that the community has the most up-to-date information, and we've linked them below for your reference.

- [Video Presentation](#)
- [Video presentation.com_subtitle.es Español](#)
- [Brochure](#)
- [Construction Options Considered Overview](#)
- [Project Overview Exhibit Boards](#)
- [Construction Overview Exhibit Boards](#)
- [Metra Overview Exhibit Boards](#)
- [Community Voices Exhibit Boards](#)

At the open house, the community was encouraged to provide comments that would be shared with the Project design team, the city, and the Alderman's office to inform the design efforts for retaining walls and fencing and potential options to restore vegetation in the public way. To submit your comments on these elements online, click [here](#). For additional project comments, please submit via the Project email address: UPNRebuild@metra.com.

Comments are encouraged to be submitted by May 23 to be incorporated into the environmental review process and for design considerations. All materials are submitted through a federal review process.

The [Frequently Asked Questions](#) page will continue to be updated in order to address questions and comments that are received.





Don't miss anything! Click here to view the full newsletter in your web browser.

Don't miss anything! Click here to view the full newsletter in your web browser.

Hello neighbors,

Hello neighbors,

Street sweeping season officially kicked off last Friday. You can find a copy of our Ward's sweeping schedule [here](#). In recent months, many of you have contacted my office urging that the City find ways to expand sweeping services throughout the year. I recently summarized and sent these requests to the Inspector General's Office, which is auditing Chicago's sweeping program later this year. I'm hopeful that the audit, once completed, will identify concrete actions for improving sweeping that my colleagues and I can act on quickly.

Earlier this week, I hosted a community meeting to provide an update on the mixed-used development proposal for 4715 N Western Ave (at the intersection of Western and Leland). In recent months, City Departments have demanded that certain changes be made to the proposal. These changes would require between \$1.3 and \$1.7 million in additional public funds, would not create any additional affordable housing, would reduce the number of parking spaces available to the public, and would risk worsening pedestrian safety in the area.

The Cubs' season officially kicked off yesterday, which means LV2 parking restrictions are now in effect. My thanks to Michael Sewall, our Director of Constituent Services, for running point on the distribution of these seasonal permits. If you are in need of one, please swing by our office at 4243 N. Lincoln Ave, or email us at info@aldermanmartin.com.

As background, the proposal includes one floor of commercial space, one floor of parking, and three floors of affordable housing. Three local neighborhood associations and the local chamber of commerce provided letters of support for the proposal. In December of last year, the Mayor's Office and Department of Housing announced that the City would award affordable housing tax credits so that the project could be financed. Earlier this year, however, I learned that the Department of Planning and the Department of Housing are demanding that significant changes be made, and as a result, the project is at risk of not moving forward.

Earlier this week, my office hosted the first in series of community visioning meetings regarding the Ravenswood Industrial Corridor (with a focus on the blocks between Irving Park and Lawrence). You can watch a recording of our meeting [here](#) and fill out a short community survey [here](#). Staying on the topic of development, 40th Ald. Andre Vasquez and I joined the Lincoln Square Ravenswood Chamber of Commerce this week to provide updates on various proposed and approved infrastructure and development projects across the broader Lincoln Square community. Finally, I'm hosting a community meeting next Wednesday at 7pm to provide an update on the mixed use, affordable housing proposal for 4715 N. Western Ave. You can register for this virtual event [here](#).

I respect my colleagues at the Departments of Housing and Planning and recognize that we all share goals of expanding access to affordable homes, supporting local businesses, and improving pedestrian and bike safety across Chicago. Nevertheless, in making decisions to further these and other goals, we must always take into account the site-specific details of every proposal; after all, no two sites are mirror images of one another. Here, the Departments' demands, while well-intentioned, risk creating more problems than they solve.

On Wednesday, my office hosted its first Open Wall event in over two years, featuring artwork provided by local nonprofit Awakenings. In conjunction with Sexual Assault Awareness Month, I'm proud to help spotlight the artistic expression of survivors of sexual violence—an issue that sadly has worsened since the Covid-19 pandemic began two years ago.

The Department of Planning is insisting that the second-floor parking spaces be accessed from the alley that runs between Lincoln Ave and Western Ave, thereby reducing the overall number of parking spaces from approximately 50 to 30. The developer and nearby residents and businesses have expressed serious concerns about this demand. They have noted that delivery trucks already use the alley for 3-11 hours every day of the week to drop off and pick up goods from multiple businesses located on Lincoln Ave. They have also noted that increased alley traffic could worsen pedestrian safety along this busy stretch of Leland, and that the City would need to allocate an additional \$1 million in public funds to cover the additional construction costs. Critically, reconfiguring the parking entrance in this way would not result in the creation of additional affordable homes.

Earlier this week, my staff and I attended local school council meetings for Lake View High School and Coonley Elementary School. Topics of conversation focused on the school-specific budgets for the 2022-23 school year, as well as ongoing efforts to strengthen community engagement (particularly with prospective students and families). In addition, the 47th Ward Green Council hosted a webinar on reducing plastic pollution in local restaurants. My thanks to ward resident Maru Braemer of MB Consulting for presenting, as well as Organizing for Plastics Alternatives and local cafes Beans & Bagels and Spoken Cafe for sharing the work they've done to reduce plastic waste.

In addition, the Department of Housing is demanding that no matter how many total parking spaces there are, an equal number be made available for the public and for residents in the building. Businesses, residents, and the developer have expressed reservations about this demand, too. They have noted that the proposed building's close proximity to the Western Brown Line Station and buses on Western Ave and Lawrence Ave will mean that many building residents will not have cars. In addition, recently provided parking-utilization data shows that the current parking lot is well-utilized during peak business hours on weekday evenings as well as weekend mornings and afternoons. Finally, reducing the number of publicly available parking spaces while increasing the number of residential spaces will cost approximately \$300,000-700,000 in additional city funds.

I hope you all have a dry and restful weekend.

I will continue working with community stakeholders to find a compromise that our community, the developer, the Departments, and the Mayor's Office can all support. With this mixed-use proposal, we have an opportunity to replenish some of the affordable housing that our community has lost in recent years while supporting our local businesses. Let's not squander it.

Yours in service,

You can watch a recording of the meeting [here](#), and can contact my office at info@aldermanmartin.com or (773) 868-4747 with any questions. I hope you all have a restful and enjoyable weekend.

Matt

Yours in service,

Matt



UP North Rebuild Open House
Learn about Metra's UP North Rebuild: Fullerton to Addison Project at a public open house from 6-8 p.m. Wednesday, April 27 at Burley Elementary School, 1630 W Barry.
[Click here to register for the event, and learn more about the project here.](#)



UP North Rebuild Open House
Learn about Metra's UP North Rebuild: Fullerton to Addison Project at a public open house from 6-8 p.m. Wednesday, April 27 at Burley Elementary School, 1630 W Barry.
[Click here to register for the event, and learn more about the project here.](#)

Public Meeting #2 Media Coverage

1) Inside Booster – April 13, 2022 – hyperlocal print publication

INSIDE PUBLICATIONS

APRIL 13 - APRIL 19, 2022 • 9

Metra to hold April 27 open house on UP-North bridge work

Metra will hold a public open house on April 27 in Lakeview to inform adjacent property owners and the public about its progress on the UP North Rebuild: Fullerton to Addison Project, which will replace aging bridges over 11 streets between Fullerton and Addison along the UP North Line.

While the project, which is currently estimated to cost \$262.3 million, is still in the design and planning stage, the project team will be able to share new information on a proposed process to modernize the bridges, the environmental review and analysis summary, project renderings and more.

The open house will take place on Wednesday, April 27 from 6 p.m. to 8 p.m. at Augustus H. Burley Elementary School, 1630 W. Barry Ave. With the open house format, attendees can stop by anytime to get information on the project and ask the project team questions.

"This project will modernize our bridges and the line's infrastructure for the long-term benefit of the community and My Metra riders, and we're committed to keeping the public informed during every phase of the process," said Metra CEO/Executive Director Jim Derwinski.

"In addition to this open house, which is our second such event for the UP North Rebuild Project, we are continuing to meet with aldermanic offices, public agencies and adjacent property owners to discuss the work. We also plan to host a third public meeting before construction begins."

where needed, and refurbishing the existing Lincoln/Addison bridge. The project would also benefit the community through improved bridge underpasses with state-of-the-art lighting. Rail customers would benefit from a smoother ride, increased operating efficiencies and reduced operating costs.

To maintain rail operations and minimize disruption to the community, the project will be built in three stages, with construction anticipated to start in 2023 and be completed by 2027.

To register for the open house, visit <https://www.eventbrite.com/e/metra-up-north-rebuild-fullerton-to-addison-project-open-house-tickets-300796920687>. Registration is encouraged, but not required.

To learn more about the Metra UP North Rebuild Project visit <https://metra.com/UPNrebuild>.

Open house
Wednesday, April 27
6 p.m. to 8 p.m.
Augustus H. Burley
Elementary School
at 1630 W. Barry Ave.

Letter to the Editor

The problem with bike lanes is just the paint

Regarding your March 23 coverage on bike lanes for Broadway, cars vs. bicycles is not a zero-sum choice.

Rules of the Road already has enough traffic laws and regulations to enable all users to share the road safely and comfortably. All that's needed is for us to obey these rules - and for police to enforce them. There's no need for restrictive lane markings and rubber bollards on our streets.

Buses aren't exempt from the problems caused by these new markings. There's a stretch of Clark St. in Andersonville where there's literally not enough width [in the traffic lane] for a CTA bus to travel safely on the right side of the street without encroaching on the marked bicycle lane. In real roadway inches and feet, there is enough room for a bus and a bike to travel side by side safely, just not according to the markings.

The problem is just the paint.

Jean SmilingCoyote

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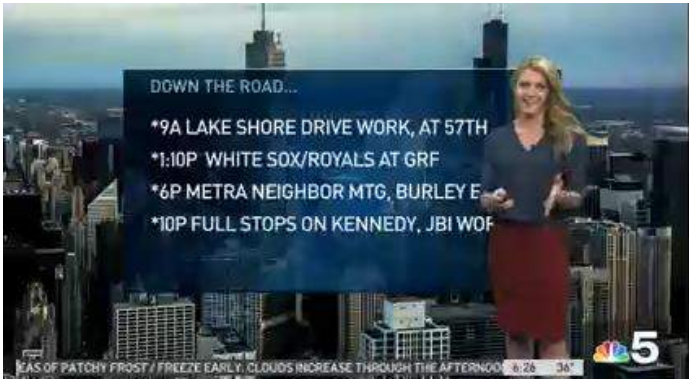
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2) WMAQ-TV (NBC) – April 27, 2022 – local news segment



3) **Block Club Chicago** - April 27, 2022 - local online publication, e-Newsletter, and social media shares

Lakeview Residents Say Metra's UP North Bridge Overhaul Will Put Trains Too Close To Their Homes

The work, which shifts tracks 20 feet west, would leave a foot of space between neighbors' property lines and Metra's retention wall.

Jake Wittich
8:00 AM CDT on Apr 27, 2022

LAKEVIEW — A group of Lakeview neighbors want Metra to reassess its \$25.3 million overhaul of the Union Pacific North Line that will take up land residents have been allowed to use for years, move trains closer to their homes and destroy vegetation.

Metra will **replace 11 bridges** between Fullerton Avenue and Comela Street while modernizing the railroad and to retaining walls. The bridges are 120 years old and have reached the end of their useful lifespan, Metra spokeswoman Michael Gillis said.

The work, expected to run in multiple phases from 2023 to 2027, is part of a larger project to modernize the entire UP North line. Metra has already replaced 11 bridges from Grace Street to Belmont Avenue and started reconstructing the line's Ravenswood Station, 4800 N. Ravenswood Ave.

Other project elements include lowering Fullerton Avenue and Comela Street to maintain current clearance under the CTA Brown Line, rehabilitating and painting the Lincoln/Adlai bridge and utility work, Gillis said.

The project will also move the railroad tracks 20 feet west onto Union Pacific-owned property next to several backyards, taking over the space neighbors have long used for gardening.

"And what's most frustrating about this to me is this is all taxpayer funded, and they're choosing the most destructive plan to achieve their end goal," said Christa Calmeyer, who lives directly west of the tracks.

Metra will answer questions and collect feedback on the project during an open-house 6-8 p.m. Wednesday at Burley Elementary School, 1630 W. Berry Ave.

The tracks are shifting west because the bridges are being replaced in multiple stages so there won't be any service disruptions along the UP North line, Gillis said.

There are two separate track lines for trains going in opposite directions, but a third track line will be built to the west tracks has been modernized, the third, older track line, will be demolished.

"This would allow Metra to maintain two-track operations during the planned five-year construction period," Gillis said. "The option is preferred as it minimizes impacts, would not require the permanent acquisition of properties, minimizes costs and is the most feasible in terms of engineering, construction and duration of work."

The work would leave 1 foot of space between neighbors' property lines and the retention wall and 13 feet of space between property lines and the railroad's nearest track centerline, Metra officials said.

"The big picture is closer is bad," said Craig Gunderson, who lives west of the railroad.

The concrete retention wall would be 9 to 13 feet tall and have a fence atop it, Gillis said. In many places, the wall would replace the existing retaining walls, which are deteriorating. The final design, height and exact locations of the walls are under consideration, Gillis said.

"At this week's public open house, the project team will be seeking feedback from the community about their preference for the fence options or patterns of the proposed retaining walls and fencing," Gillis said.

Neighbors want Metra to explore alternatives to replacing the bridges that don't involve shifting the tracks west.

"We absolutely understand that the bridges need to be replaced, but moving a train that close to my home and eroding the wall is not necessary to achieve that goal," Gunderson said.

One alternative neighbors have suggested is a method known as rolling in the bridge, which involves building each of the 11 bridges next to the existing ones and rolling them into place during track outages, according to Metra.

But that strategy would further affect rail service because temporary earth retention and short temporary bridges known as jump spans would need to be constructed at each location, Gillis said.

Rolling in the bridges also would increase construction costs and the duration of the project due to additional construction stages that would be required, Gillis said.

"This option was eliminated as it would require multi-day train service outages and delays at each bridge location and would require more construction work on adjacent properties," Gillis said. "This option is less feasible than others from an engineering and constructability perspective."

Brenda Ilario, another neighbor who lives west of the railroad, said she was most concerned about losing the trees and vegetation on the west side of the tracks and having trains' fumes even closer to her home.

"Those diesel fumes go past our homes and linger in the air, and it's really scary to me, but I always felt better because I had so many feet of trees between me and the tracks," Ilario said. "I'm hoping we can kick up enough noise to show [Metra] that they can treat us as neighbors and be more considerate of us as the community that they're coming through."

The group of neighbors wants Metra to take their concerns into consideration and rethink how the bridges will be replaced to minimize impact on their homes.

"They could do a lot more for us than they're offering," Calmeyer said. "The fact that they're not even willing to give us an extra foot or two to put up some absorbent and some plants is just absurd."

Block Club Chicago
Your Daily Dose of Neighborhood News
April 27, 2022

The CTA Boosted Security To Battle Violence, Bad Behavior On Trains And Buses — But Riders Say Nothing Has Changed

The CTA has hired two private security firms to monitor the location of stranded people pending the system's fix. But some people say they'll be equipped to deal with violent or unruly passengers.

JEFFERSON PAUL
Ald. Jim Dardar's Ward Superintendent Arrested, Charged With Trying To Sell Illegal Machine Gun

An Chicago 23-year-old man being arrested, he reportedly asked to transfer Dardar before he was placed into custody, according to law enforcement, which was obtained by The Tribune.

POLICE THREATEN
Next Stop: Polish Triangle, Wicker Park's Division Street Blue Line Station Could Be Renamed To Honor Area's Polish Past

A proposed issue all. Brian Rogovin would rename the station "Division Polish Triangle." The idea was now before to leadership of Polish businesses and organizations.

LAKEVIEW
Lakeview Residents Say Metra's UP North Bridge Overhaul Will Put Trains Too Close To Their Homes

The work, which shifts tracks 20 feet west, would leave a foot of space between neighbors' property lines and Metra's retention wall. The railroad moves the property.

OLD STOREY PLACE
The 'Tomato Men' Is Back With More Than 4,000 Tomato Plants For Sale This Weekend

Chicagoans wanting to grow their own tomatoes can get in line from 10 a.m. until midnight on Saturday and pick them up Saturday at the North-West Side pop-up event.

Block Club Chicago
April 27 at 11:05 AM

The work, which shifts tracks 20 feet west, would leave 1 foot of space between neighbors' property lines and Metra's retention wall.

buff.ly/3KlrESq

294 Comments 44 Shares

Block Club Chicago
@BlockClubCHI

Lakeview residents said Metra's UP North bridge overhaul will put trains too close to their homes.

buff.ly/3KlrESq

1:00 AM · Apr 28, 2022 · Buffer

1 Retweet 10 Quote Tweets 15 Likes

- 4) **WBBM-AM – April 27,2022 – a total of 5 on-air mentions and stories aired throughout the day during news segments**
- 5) **WBBM-TV (CBS Chicago) – April 27, 2022, and April 28, 2022 – local news segment and online publication**



CBS CHICAGO NEWS WEATHER SPORTS VIDEO MORE 77°

West Lakeview neighbors say reconstruction of Metra Union Pacific North tracks will ruin their backyards

BY SABRINA FRANZA

CHICAGO (CBS) – Neighbors along the western edge of Lakeview are talking with Metra about a construction plan they say will destroy their yards.

As CBS 2's Sabrina Franza reported Wednesday, no one we talked to is debating that the bridges along the Metra Union Pacific North Line at Ravenswood Avenue need replacing. It is the way to do it that is up for debate.

The West Lakeview neighbors were set to meet Metra face-to-face Wednesday to talk about the issue.

Christie Calixte had her wedding in the backyard of her home shutting the tracks.

"We thought this was our forever home," she said.

Calixte's two sons also now play in the yard.

"We bought this home knowing the railroad was here, but this scope of the project was inconceivable," she said.

The Metra tracks are a few feet east of her home. They lie on top of a low wall made of masonry stone masonry with greenery alongside the tracks, and fence such as and decays mounted atop the stone.

Starting in the summer of 2023, Metra plans to replace the walls of the railroad embankment, and the tracks, in a \$242 million taxpayer-funded project.

A total of 11 bridges on the UP-North Line will be reconstructed, from Fullerton Avenue on the south to Addison Street on the north.

"We are going from a really wide open space – you know, a nice buffer between our home – with green space – our home between moving trains, to an imposing, sterile, prescriptive-style cast concrete wall," said Craig Gooderson.

Indeed, the masonry wall that is set to be erected as part of the reconstruction is the issue for the neighbors.

"The wall that will be here will be 16 feet tall," Calixte said.

Gooderson and Calixte's worry about possible unavoidable damage to their property.

"Our landscaping will be destroyed," Calixte said. "Our yard will be destroyed."

Metra spokesman Michael Gillis explained what the goals of the project are.

"We have two pretty main goals," he said. "One of them is to replace the bridges, obviously. The other is to do that in a manner that allows us to keep the line operational."

As Gillis explained, the current structure allows space for a third track. A new track will be built in that spot, then an old one will be replaced, and finally, the oldest track will be removed.

With track methods involved, service will not be disrupted.

Neighbors say leaving a space with an option for a third track later on is what is keeping Metra from finding a way around the construction disruption.

Gillis: "We are not adding a third track."

Franza: "But are you going into it with the mindset of leaving out people who live in the neighborhood and saying, 'Maybe we'll be taking some of their suggestions?'"

Gillis: "Well, we are listening to the community, and we are soliciting their suggestions about the look of the retaining walls and the look of the fences. We're really here to just tell them why we're proposing to do what we're doing."

The project isn't set to start until summer 2023. Metra estimates it will take at least five years to get it done.

Union Pacific Rail owns the tracks and pointed us to Metra for comment.

A town hall meeting on the Metra plan was in progress Wednesday evening at Augustus H. Buckley Elementary School, 1630 W. Barry Ave. Neighbors said they were not able to talk to Metra representatives directly at the meeting, but had to submit comments ahead of time.

CBS Chicago April 27 at 8:15 PM

"Our yard will be destroyed."

CBSNEWS.COM

West Lakeview neighbors say reconstruction of Metra Union Pacific North tracks will ruin their backyards

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Chicago Tribune
Metra eyes long-delayed bridge work on Union Pacific North Line

By Sarah Freishtat
Apr 28, 2022 at 3:02 pm

Metra is preparing to replace nearly a dozen aging rail bridges along a stretch of Chicago's North Side, marking the long-delayed next phase of a bridge project that began more than a decade ago.

Metra officials say their preferred plan for the project on the busy Union Pacific North Line would mean minimal changes to commuter train schedules. But property owners who live near the project raised concerns that the construction plan will harm them and their homes.



The \$262 million project involves replacing 11 bridges, nearby track and retaining walls along the Union Pacific North Line between Fullerton Avenue in Lincoln Park and Cornelia Avenue in Roscoe Village. The bridges are around 120 years old, and are beyond their useful life span, Metra has said.

The project is still being designed, but Metra's preferred option involves shifting the tracks about 20 feet to the west within the existing railroad property, in a series of moves that will allow service to continue on two tracks during construction.



Underpasses would get new lighting and improvements, and Roscoe Street and Cornelia Avenue would be lowered to keep the current clearance between the streets and the new, larger bridges, which would be slightly raised in most other places. A bridge at Lincoln Avenue and Addison Street would be refurbished and painted, but not replaced.

Construction is expected to begin in summer 2023 and last five years.

The work is part of a broader effort to replace 22 bridges along the line and rebuild the Ravenswood station. It was initially set to begin in 2010 at a cost of \$485 million at the time, and last eight years.

The first part of the work was delayed about a year after Metra tried to reduce service to a single track during construction, wreaking havoc for commuters and leading to complaints about late and crowded trains, the Tribune previously reported. The agency eventually revised plans so it could keep two tracks in service and avoid major schedule changes, raising the expected cost.

Years later, parts of the project were again delayed by budget issues, partly linked to state funding concerns at the time and partly to the expense of installing a federally-mandated train safety system, the Tribune has reported. Work on the first 11 bridges farther north along the line has been completed, and the Ravenswood Station construction is expected to be finished this year, Metra spokesman Michael Gillis said.

Metra learned its lesson from the earlier attempt to reduce service to a single track for the first 11 bridges, Gillis said. The agency considered reducing service to a single track this time around, as well as other construction options, but deemed they would lead to delays and higher construction costs.

If it moves forward as planned, the new round of construction work is not expected to interrupt service during weekday rush periods. Slow zones might be needed at times, Metra said, and trains could be funneled onto a single track outside of rush hour.

Metra recently tweaked scheduled on the Union Pacific North Line, including eliminating four midday trains, though Gillis said their elimination was largely tied to a different construction project. The number of trains the agency is now running on the line is comparable to 2019 levels.

The construction comes as Metra looks to continue reclaiming riders. One day this week, Metra provided 14,500 passenger trips on the Union Pacific North Line, about 42% of pre-COVID-19 levels. Gillis said Metra hopes more riders will have returned by the time construction starts.



'We hope (nearby homeowners) understand that the bridges need to be replaced, and we need to do it in a way that the line is still operational,' Gillis said.

That was one point of contention for homeowners who packed a recent public meeting, asking questions and grilling project leaders during a heated discussion. Some said Metra's focus should not be on disrupting service for commuters in the short term at the expense of the long-term effects on their property.

Sharl Ludwig was among a group of homeowners who live along the west side of the train line who are upset with the construction plan. She said her property backs onto the tracks, and she worried about the effects of increased vibrations on her century-old home and fumes from diesel engines that would be running closer to her house.

Ludwig also worried that putting up a new retaining wall will block her route out of her backyard in an emergency, she said.

'It's going to change a lot of people's lives, and most of them not for the good,' she said.

Joe Hesse, who lives on the east side of the tracks, said he feels bad for people on the other side of the tracks who will have trains running closer to their properties. He also worries about noise, traffic and street closures throughout the area for years during construction.

He and his neighbors have planted a garden along a swath of the railroad embankment in an effort to clean up and beautify the area, and he imagines at least some of it will be destroyed during construction. Still, he understands the need for the project.



'They have to redo the bridges,' he said. 'They're falling apart.'

Union Pacific owns the train tracks, but Metra is on the hook for the project because the stretch of tracks is not used by freight trains, Gillis said.

For decades there were three sets of tracks on the line, but the third set was removed around 1980 by the owners at the time. Metra does not intend to add a third set of rails but will maintain space for them after the project.

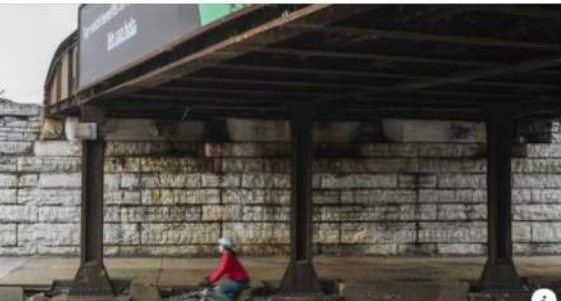
Metra is also undertaking bridge work elsewhere in Chicago, Gillis said. In one instance, bridges farther north on the Union Pacific North Line near Peterson and Ridge avenues will be replaced as part of construction of a new station.

Other bridges being rebuilt this year include one on the Milwaukee North Line on Chicago's Northwest Side and on the Rock Island line on the South Side. Repair work is taking place on Metra Electric bridges.

The Union Pacific North Line has a particularly large number of bridges through Chicago compared with other Metra lines, Gillis said.

Chicago Tribune
April 29 at 8:12 AM

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7) **Streetsblog Chicago** – May 2, 2022 – local online real-estate/construction publication and social media shares

STREETSBLOG CHICAGO

Mutiny at the meeting: Lakeview neighbors hijack Metra hearing to voice UP-N concerns

By GUY THORNTON, MAY 2, 2022

Last Wednesday's community meeting about the \$262.3 million rehab of the portion of the Union Pacific North Line that runs through the busy Lincoln Park and Lakeview neighborhoods was supposed to be an open house format. But the roughly 40 residents in attendance insisted on a town hall-style format, where they could publicly voice their concerns that the project will impact their quality of life and lower property values, and they refused to take no for an answer.



Metra and Union Pacific Railroad, which owns the rail line, are currently developing a plan to replace bridge, shore up embankments, and improve viaducts between Fullerton Avenue (3400 N.) and Addison Street (3600 N.), but the most contentious aspect of the project is that Metra and UP plan to shift the railroad tracks west, closer to local homes.

Background
The CP-N line previously had three tracks, but the westernmost one was removed in the 1980s. Metra is planning to do what it previously did between Grace Street (3300 N.) and Fullerton Avenue (3400 N.) in Edgewater and Uptown. There the railroad built a new west track, then replaced the center part of the bridge that would hold the new east track, and then removed the old east track section. With this method, there are always two tracks in service throughout the entire process.

As Streetsblog has previously reported, the current plan calls for replacing 11 railroad bridges, refurbishing the new Addison Lincoln bridge, improving the viaducts, adding lighting, and making the sidewalk wheelchair accessible. The project will also lower Riverside Avenue (3400 N.) and Cassella Street (3100 N.), which are located south and north of the spot where the CP-N Branch Line passes over the Metra tracks. In addition, the work includes refurbishing the embankment structure and adding retaining walls on the west side of the embankment, plus a few sections of the east side of the embankment.



Concerns about how far west the new west track will be located came up during the first meeting about the project last September. Neighbors became more worried when Metra gave the preliminary answer earlier this year. The center of the westernmost track will be moved 20 feet west, that is, 13 feet east of the west edge of the embankment, and the retaining walls will be a foot from the neighbors' property lines.

The new infrastructure will stay within the current Union Pacific right of way. However, the project will put trains closer to many homes and businesses, and some neighbors who live so close to the CP-N line that an extension of their backyard may mean that private lines also abut their lots may need to temporarily occupy some private property to stage equipment.

People who live directly west of the tracks recently told **Block Club Chicago** that when they bought their homes, they knew the railroad had the right to move the tracks further west, but indicated they didn't expect that would actually happen.

Moreover, some neighbors have been gathering on railroad property next to their homes, even though they understand it won't cause harm.

For example, below is a before-and-after rendering provided by Craig Gunderson, a resident of Piccadilly Place. That's a row house development north of Riverside Parkway (2300 N.), just west of the tracks, which [you can read about here](#) described as "a highly desirable private gated community."

The confrontation
The open house took place in two different rooms, with the south room used to share project details and the north room used to discuss the preliminary results of environmental studies. According to the [presentation handout](#), the study "preliminarily determined that the project would likely not have significant adverse environmental impacts." That doesn't guarantee that it won't, just that it probably won't based on preliminary findings. The Noise and Vibration Analysis found that the predicted increase would be within 1.7 - 2.3 decibel range, which falls well within the "acceptable" noise range.



As the meeting went on, neighbors gathered in the western room to demand that the project staff hold a town hall. When they made their case, neighbor Brenda Barrie led the charge. The project staff initially stood their ground, and at one point a Metra police officer tried to persuade the crowd to back off. But the officials eventually relented and agreed to set up a panel in the south room.

As the town hall got underway around 6:50 p.m., Barrie asked several questions from a prepared list, which focused on what rights the residents have to challenge the project, and who makes the decisions.

The residents expressed concerns that the changes would increase their exposure to train noise and diesel pollution. They also said they're worried about flooding and the removal of trees that grow on and beside the embankments.

According to the **Block Club report**, neighbors had previously asked Metra to use an alternative bridge replacement method called "rolling in the bridge." This would essentially build the new spans next to the existing ones and installing them during service shutdowns. But that approach requires shutting off CP-N service on multiple days and delays at each bridge location, as well as increasing the cost and length of the project, Metra spokesperson Michael Collins told **Block Club**.

But at Wednesday's meeting, the resident maintained that a few years of inconvenience for thousands of Metra users was less important than the permanent impacts to their properties caused by the change in track location.

They asked the committee railroad on why they couldn't replace each bridge all at once, instead of one by one at a time, pointing to the South Shore Line's [1,000,000-pound bridge replacement](#) as an example of how it could be done. Justin Patton, the engineer for the project, said the South Shore Line had much more room to work with in that industrial section of Highland than they will in dense, mostly residential Lakeview.

Steve Hanks, the environmental consultant for the project, said that, under the current safety standards, the railroad tracks need to be built farther apart than they did when the line was originally allowed.

Several residents complained the project will lower their property values and hurt their community. Almond Nagebi, who has lived in Piccadilly Place for the past four years, said that, after the original west track was removed, much of the industry around the embankment gave way to residences. "We've been growing, gentrifying, changing the North Side, and you're endangering everything we have done," he said.

As the meeting wound down, residents pressed the project staff to commit to holding another town hall. Sullivan said that they didn't have the authority to do that. "This is outside the scope of our current consulting contract. I don't have the authority to change the contract unilaterally."

Home thoughts from residents
My interview with the residents who pushed for a town hall revealed certain patterns. Most of all of them moved to their current homes over the last few years, and even the ones who have been there longer moved in long after the original west track was removed. A significant portion of them live in Piccadilly Place.

Craig Gunderson, a Piccadilly Place resident of three years, complained that project would turn his backyard into a "meat," increase train noise and "rip up the green space." He added, "I would like to see Metra and U.P. fix this space with more consideration. I don't believe they need to come as far west as they [believe they] do."

Metra didn't describe the fence for last week's meeting in detail in an email or either the [press release](#) or the [project page](#). Neighbor Christine Calverton told me residents have been demanding a town hall since the September hearing, but Metra declined to hold one. She added that the "vague wording of the info preceding last week's event gave them hope that the committee railroad might hold a town hall." "We were not surprised - we were more disappointed" with the actual format, she said.

Calverton told me she and her husband bought their century-old home on Barry Avenue (3100 N.), just west of the CP-N, specifically so their children would have a space to play. "Over 100 homes are impacted by this, and we all have children, and we all have pets. We have a one-story, 20-year-old that's on our property, and our advisor told us they're going to tear it down."

Out of all the residents I interviewed, only Calverton told me she uses the Union Pacific North Line, saying she takes it to see her brother-in-law in North Shore suburbs of Villa Park. She argued Metra would be better off realigning the money from this project to convert locomotives from diesel to hybrid. But even if it wasn't necessary to replace the 125-year-old bridges, which is, the \$116.1 million earmarked for the project from the federal stimulus tax infrastructure bill couldn't be used for locomotive conversion, because that funding was specifically appropriated to replace bridges.

After the meeting, Barrie struck a conciliatory tone, saying that she hopes that Metra will work with neighbors on a compromise, and that "Metra can be a good partner and not destroy property and neighborhoods."

Streetsblog Chicago
May 2 at 10:46 AM

Lakeview residents, many of whom live in "a highly desirable private gated community," hijacked a Metra meeting to demand that the UP-N rehab plan be changed to prioritize their property values over avoiding service outages.



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Residents from "a highly desirable private gated community" abutting Metra's UP-N line are unhappy with the bridge replacement plan and would like to speak with the manager.



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